



August 2018

UDA

**STANDING AT THE CROSSROADS
— DESIGNING A GATEWAY FOR
THE FUTURE**

Dorchester County, South Carolina

GATEWAY TO A HERITAGE TOURISM REGION: COOKS CROSSROADS

PREPARED FOR

Dorchester County, South Carolina

FUNDED BY

**Dorchester County
Dorchester Trust Foundation
South Carolina National Heritage
Corridor**

CONSULTANT TEAM

Urban Design Associates

The intersection of SC 165 (Bacons Bridge Road) and SC 61 (Ashley River Road) is known as Cooks Crossroad and constitutes a remarkable opportunity for a gateway for tourism into the Ashley River Region and to Summerville and Charleston. The challenge now is to see that good design happens there, a design that gives a sense of place. What none of us want is something like the cookie-cutter developments seen along too many roadways in suburban South Carolina.

This place is different for within a short distance are a remarkable combination of scenic and historical resources for tourists and residents, a combination unrivaled in the state and perhaps in the nation. However, to be effective, a sensitive and well-crafted plan needs to be produced for Cooks Crossroads. Otherwise, one might see well-intended but confusing or mediocre designs, and such critical things as design cohesion, connectivity, stormwater management, complete street traffic management, landscaping, sign controls, lighting, and other key elements of planning may not happen.

The good news is that Dorchester County's Planning and Zoning Department cares, along with a host of private citizens and public officials. Why is this important? The answers are to be found in the place itself. For one, this region itself is a key component of the SC National Heritage Corridor, a nationally recognized Corridor stretching from the South Carolina mountains to the coast, and SC 61 is a major route for that corridor, almost serving as its spine. As a result, this intersection, if properly planned and designed, could offer opportunities later for an orientation center for tourists and residents as well as for the Ashley River region and the SC National Heritage Corridor. Strategically placed, such a facility could fill the current void, so tourists and residents could get a graphic overview of the region as a whole and also learn of its historical and natural components. But before that can be effective, a plan for the district is needed.

The intersection also marks the terminus of the Ashley River Road as a National Scenic Byway, which continues towards Charleston for 11 miles. In the opposite direction, the road is known as Beech Hill Road, but it's still the route first traveled by Native Americans as they traveled back and forth between the coast and the interior.

Much of that Ashley River Region is designated as Historic Districts on the National Register. The Ashley River is a State Scenic River, with a soon-be-constructed landing for canoes and kayaks just downstream from Bacon's Bridge itself on the new county parkland, which the SC National Heritage Corridor has provided key funding. As visitors travel from Cook's Crossroads down the Ashley River Road towards Charleston, they will encounter: Middleton Place, a National Historic Landmark and home to a signer of the Declaration of Independence; Magnolia Plantation and Gardens, established in the late 1600s and among the finest romantic gardens in the nation; Drayton Hall, a National Historic Landmark and an icon in American architecture. All were home to generations of Carolinians, both white and black, before and after emancipation, and their stories are being told in more compelling ways. Beyond Drayton Hall is Old St. Andrews' Episcopal Church, dating to the early 1700s, one of the old Episcopal churches in the state and nation, and a National Historic Landmark.

Much of that Ashley River Region is designated as Historic Districts on the National Register.

At one corner of Cook's Crossroads is Rosebrock Park, owned by the Dorchester Trust Foundation and leased to the county, and open to the public with nature trails and interpretive signage. Its walking trails and river views have proved too popular with people of all ages.

On the opposite side of the Ashley River is a new Ashley River Park, owned by Dorchester County, and soon to be opened with a range of amenities. It will attract hundreds of visitors each day. Also, there is a kayak and canoe launch, funded in part by the SC National Heritage Corridor, which is a key part of the Ashley River as a Blue Trail. It will be a major contributor to ecotourism, and the Ashley offers unique opportunities for such non-motorized boat travel up to US 17-A, where there will be another kayak and canoe launch, operated by Dorchester County.

Bacon's Bridge itself is a historical area because, during the American Revolution, Nathaniel Green's soldiers were there, as were those of General Moultrie. Archaeological sites still remain for future interpretation on both sides of the river.

A short distance downstream from Bacon's Bridge Road is Colonial Dorchester State Park, a settlement of Congregationalists from Dorchester, MA, dating to the 1690s. It is an increasingly popular state park and is uncovering via archaeology a remarkable story of colonial life.

SC 165, Bacon's Bridge Road, leads into Summerville and to its historic downtown and tourism amenities. In the opposite direction, SC 165 arcs down towards US 17 and is becoming all the more a route for residents and tourists coming to work or visit in Summer's Corner, Oakbrook, and Summerville

Thousands of people, residents and tourists alike, drive to and pass through Cook's Crossroads each day. The question is: what will they see? The district will change, but how? A good design offers major opportunities. Earlier, the intersection of Dorchester Rd and Bacons Bridge Rd., about a mile away from the river, was designated as a gateway, but the vision had little support, and now two convenience stations and a strip shopping center are there. If plans for this intersection fail, there's no Plan B.

What is hoped for is that if tourists are coming up from Charleston and past Middleton Place, or coming from Summerville to Historic Plantation District of Middleton Place, etc. they could come to Cook's Crossroads, see an attractive district, be encouraged to shop or dine, and be oriented as to what to see and do in the region or along the entire SC National Heritage Corridor, or the Gullah/Geechee Corridor thereby prolonging their stay and spending more money in the state. But before any of that can happen, there must be a thoughtful plan that citizens and public officials support and that creates a distinctive and inviting sense of place.

One can Google this area to learn more its resources and to continue into Summerville and Charleston to see how this location stands at the center of a remarkable yet fragile area, replete with historical and natural resources, many of them accessible to the public. Gateway to a Heritage Tourism Region: Cooks Crossroads.

Thousands of people, residents and tourists alike, drive to and pass through Cook's Crossroads each day. The question is: what will they see?

*George McDaniel,
Chairman, Ashley Scenic River Advisory Council*

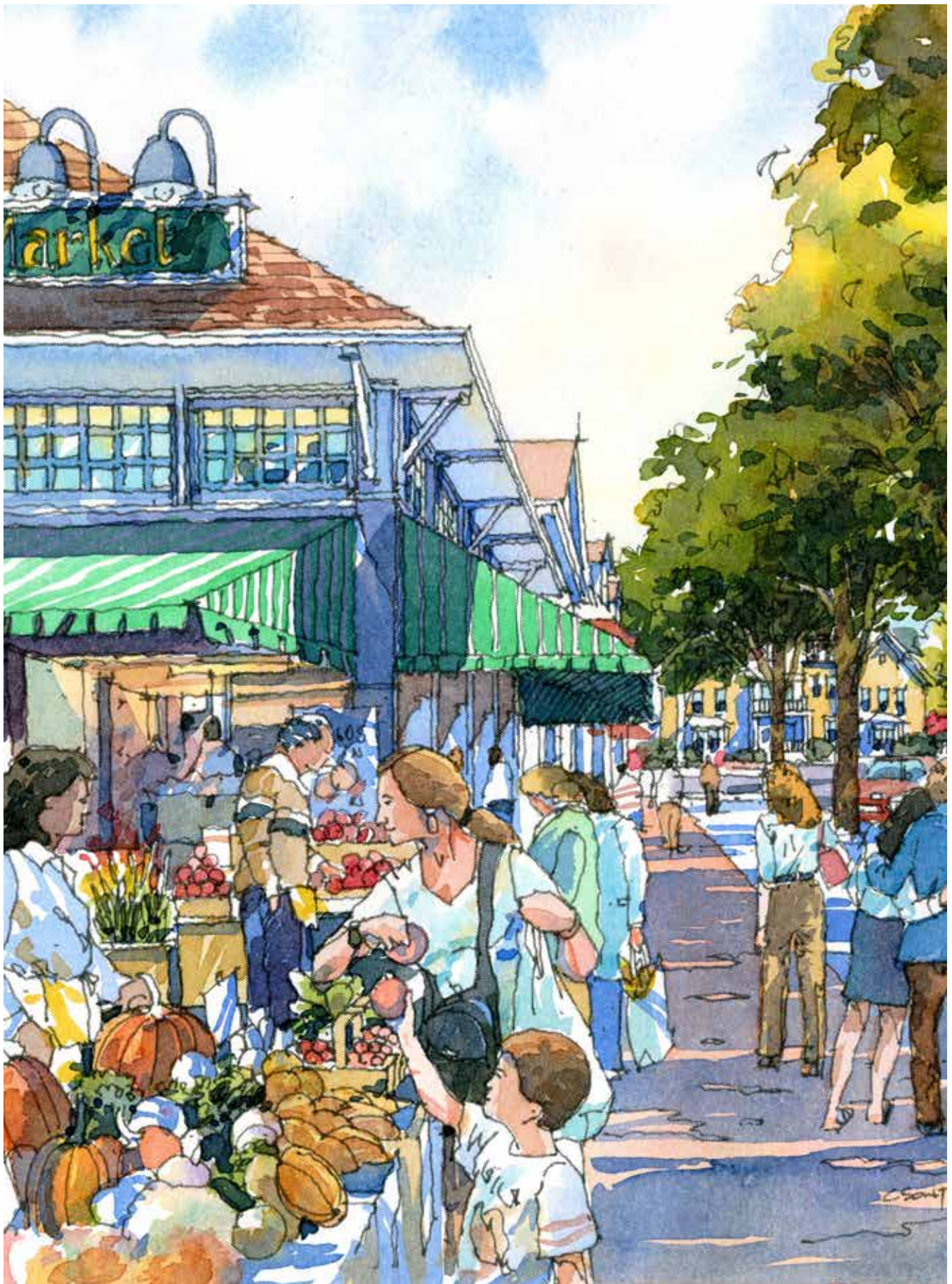


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OVERVIEW

HISTORIC CONTEXT

The Scenic Ashley River Road and Historic District constitute a unique national resource preserved for future generations as a window into the unique legacy of the region.

The 23,828-acre Ashley River Historic District corridor, listed on the National Register of Historic Places, represents more than 300 years of cultural and ecological significance for its association with the Lowcountry’s inland and tidal rice cultivation history, the Civil War, Post-Reconstruction Era phosphate mining industry, and the history of African Americans from slavery to freedom in Charleston. Stretching north for approximately twelve miles along the Ashley River from Church Creek in Charleston County to Bacon’s Bridge Road in Dorchester County and south to County Line Road, the corridor includes historic plantations and gardens, rural landscapes, churches, residences, and archaeological sites. In 2007, Historic Charleston Foundation received a grant from the Gaylord & Dorothy Donnelley Foundation to expand the Ashley River Historic District, which was originally listed on the National Register of Historic Places in 1994. The expanded district was designated in 2010 and now encompasses more than 23,000 acres. In June 2018, the National Trust for Historic Preservation named the Ashley River Historic District in Charleston County a National Treasure and listed it on its annual list of America’s 11 most Endangered Historic Places.

Expanding the Ashley River Historic District was just one part of a decades-long, collaborative effort to preserve the area’s significant natural, historical and cultural qualities. This effort is on-going as there remain severe threats to this area, but substantial progress has been made. This document provides detailed development guidelines for District 5 properties that create a western gateway to the historic district at the intersection of Bacon’s Bridge Road and Ashley River Road.



District 5 Gateway



Magnolia Plantation



Colonial Dorchester State Historic Site



Middleton Plantation



Drayton Hall



Ashley River Road



Magnolia Plantation Gardens



Ashley River

PURPOSE AND VISION

Enhancing and perpetuating the sense of place is at the core of the purpose of these design guidelines. The Cooks Crossroad must own the Lowcountry brand as a strategy for ensuring that development is context appropriate and differentiated from surrounding conventional development. The Crossroads should not be ‘just anywhere’. It presents the last opportunity to create a gateway to the unique historic and cultural assets that is derived from the extensive tradition and principles of Lowcountry architecture and landscape.

Dorchester County is growing rapidly as the region’s economy continues to attract high-quality employment uses and a significant expansion of the housing market. Arterial roads such as Bacon’s Bridge Road are experiencing increased traffic volumes as new areas of development draw additional residents and businesses. Conventional development patterns of service-oriented commercial uses have developed along the corridors replacing older agrarian uses or undeveloped land. While the development serves daily needs and supports new businesses, the character and quality of the built environment have become a collection of buildings and facilities that line the highways without a common design language that reflects the unique sense of place.

The County has developed a series of zoning overlay standards to address many of the issues that contribute to a loss of environmental quality. Sites flanking the Cooks Crossroads intersection of Bacon’s Bridge Road and the Ashley River Road have been designated as an important gateway into the historic Ashley River District. The vision is to create a detailed policy with site development guidelines including architecture, signage, lighting, and landscape to ensure a high-quality environment for generations to come at this vital gateway. The patterns detailed in this publication provide standards for development which will be administered through the Dorchester County Planning Department.



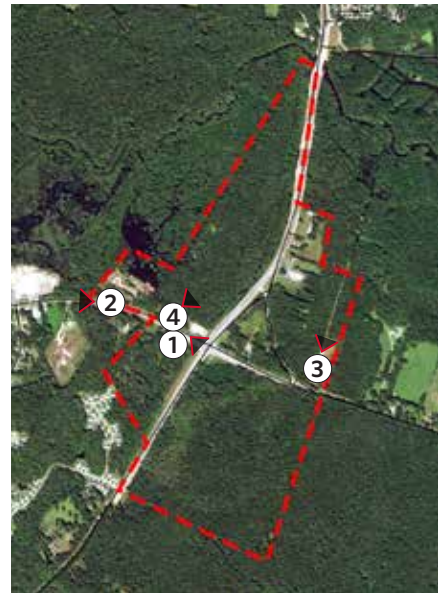
Existing aerial of study area

TRANSFORMING PLACE

- Specific landscape treatment along major arterials significantly improve the character and experience
- Creating inter-connected sites between individual properties for access and stormwater management is key to building a cohesive place
- New buildings and signage should reflect the traditional character and heritage of the region



1. Current intersection of Cooks Crossroads (SC 61 and SC 165)



2. Ashley River Road (SC 61) going west



3. Private road connecting to Ashley River



4. Trail head in Rosebrock Park

DESIGN PRINCIPLES FOR THE GATEWAY

Design Principles for the Gateway have been developed to ensure that quality design will continue to enhance property values. Conversely, ad-hoc development hurts values, and the objective is to preserve values over time. Creating a unique crossroads with welcoming character and a strong sense of place will draw people in and sustain value.

Another way to advance these principles would be through the creation of a gateway or visitor's center located at Rosebrock Park or within a building in one of the other quadrants. There is a need for such a facility to orient residents and visitors to all of the amenities of the region, including the Ashley River Historic District and Summerville; and Cooks Crossroad is the ideal location.

1. Preserve and Protect the Cultural Landscape along SC 165 and SC 61

The landscape buffers, regulated in Article 11 of the Ordinance, preserve the historical and natural character of the corridor. Restrictions on clearing vegetation and planting requirements ensure acceptable quality, quantity, and type of trees and shrubs in the buffers.

2. Access Development through Connected Shared Drives

Wherever possible, parcels should be accessed from shared drives. Accessing parcels from shared drives will minimize the number of access points that cut through SC 165 and SC 61 buffers.

3. Focus on Low-Impact Strategies and Accommodate Water

Strategies for stormwater, flood mitigation, and connected water systems are essentially critical to these development sites; water management must be maximized in this extremely sensitive area due to the proximity to the Ashley River.

4. Organize Buildings around Shared Spaces to Create a Sense of Place

In the region, commercial nodes in rural areas historically developed around shared spaces. The Design Guidelines describe appropriate spatial types. Commercial and non-residential development should be organized around one of these types.

5. Design Using Recognizable Lowcountry Building Types and Architectural Forms

Rural commercial buildings in South Carolina have simple shed roofs, identifiable massing, and a mix of residential and retail elements. New buildings should fit into the context, with additive forms and small scale.

6. Design Landscape and Site Elements to Fit the Crossroads Character

Site landscape and elements, including signage, fencing, and hardscape materials, must work together cohesively to enhance the Crossroads character.



Buffers along SC 61 and SC 165 serve the objective of protecting the cultural landscape along the major arterials.



Relationship between retail and commercial buildings, drives, and parking lots is critical to establishing a character consistent with the Cooks Crossroads area.



Cooks Crossing building types have recognizable forms and architectural character

BUILDING THE CROSSROADS

When applied together, these principles will produce development of the scale, type, and character that will fit into the Cooks Crossroads context and character. When approving development proposals, the County will evaluate submittals based on these principles and standards outlined in these Guidelines. This document will serve as a visual companion to the Ordinances. The image below shows the comprehensive result.

There are a number current conditions and real obstacles that necessitate the adoption of design guidelines and modifications to the public road infrastructure. The intersection is barren, unsafe, and unwelcoming for pedestrians, and improving the intersection will be a critical component for ensure the success of a gateway.

Cooks Crossroads will be a place that is decidedly different than the types of development that result without these intentional Guidelines. Cooks Crossroads will be a place with charm and character – walkable, and oriented towards people walking and biking, rather than cars driving. People will come to Cooks Crossroads because it will be a pleasant, unique place, not just for the uses located within (shopping, dining, etc.). This is a rare opportunity to be distinctive in an area that has not to-date produced distinctive development or reflected the Lowcountry heritage. Cooks Crossroad will capitalize on the scenic river, the parks, and the scenic highway to create a special place.

CROSSROADS STRATEGIES

GATEWAY DESIGN

- Landscape buffer preserved along Ashley River Road and SC 165, maintaining the Historical Ashley River Corridor
- Coordinated access between parcels
- Uniform site design elements (fencing, lighting, and signage) form an identity
- Minimal grading and preservation of existing terrain and vegetation

BUILDING DESIGN

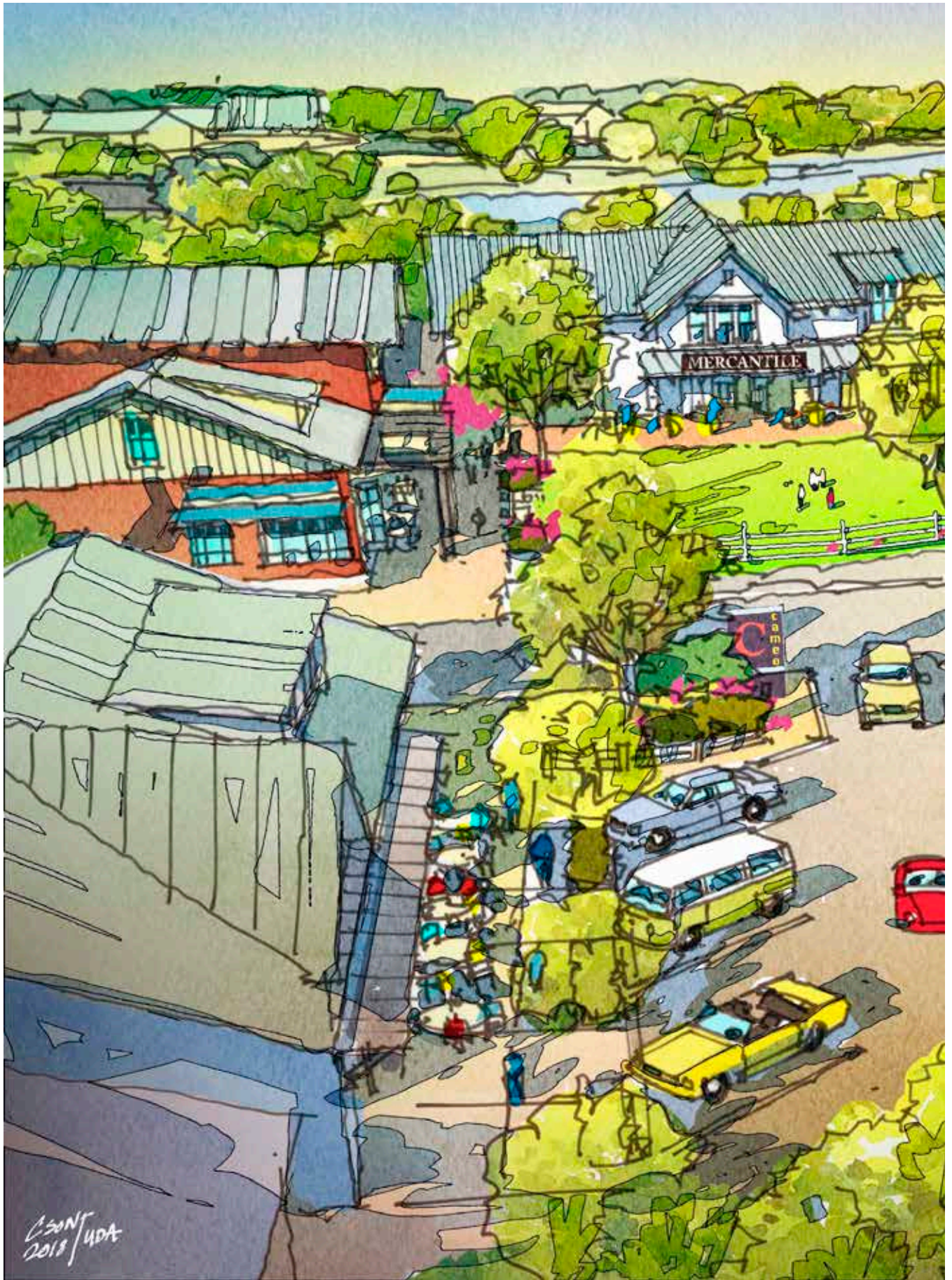
- Buildings located to frame corners, street intersections, and entry points
- Stepped building height and massing adjacent to smaller-scale development

LANDSCAPE DESIGN

- Protected Natural Water Courses and Drainage Features
- Well-designed landscape screening parking and loading from public view



Applying the principles together will produce development that fits into the Cooks Crossroads context.





GATEWAY PATTERNS

DEVELOPABLE AREA AND ACCESS

DISTRICT 5 USES

The uses below are the permitted, conditional, and prohibited uses in District 5. These regulations are intended to protect and enhance the rural and historic character of the area.

Permitted Uses

- Agricultural uses (farming, horticulture, gardens, nurseries, animal husbandry, agricultural-support services)
- Single family detached houses
- Education and religious uses
- Golf courses, ecotourism, and historical establishments
- Public safety facilities; cemeteries
- Open space

Conditional Uses

- Manufactured housing
- Cultural arts centers, private clubs, recreation
- Hotels, rural inns, and hospitality
- Convenience retail, including pharmacies and grocery stores
- Offices (professional and medical)
- Utility plants and transmission facilities

References

For specific use requirements, please refer to:

- [Article IX Zoning and Development Rural District Regulations, Section 9.2 Conservation District CV](#)
- [Article XI Special Areas, 11.5.8 Requirements Imposed on All Districts and 11.5.13 Additional Requirements for District 5](#)
- [Article VI Use Groups](#)

CONSTRAINTS



Setback Requirements

Properties in the Conservation District (CV) have a minimum 50 foot setback from all property lines.

References

[Article IX. Zoning and Development Rural District Regulations, Section 9.2.6 Minimum Lot and Building Requirements](#)



Required Buffers

A number of overlays establish landscape and environmental buffers from the SC Highway 165 and SC Highway 61 ROWs and from the edge of the Ashley River.

References

- [Article XI. Special Areas](#)
- [Section 11.2 Transitional Overlay District \(TOD\);](#)
- [11.2.6 Minimum Visual Buffer \(MVB\) Standards](#)
- [Section 11.4 Ashley River Corridor Overlay District;](#)
- [11.4.4 Supplemental Design and Protection Standards](#)
- [Section 11.5 Ashley River Historic Overlay District;](#)
- [11.5.8 Requirements Imposed Upon All Districts](#)



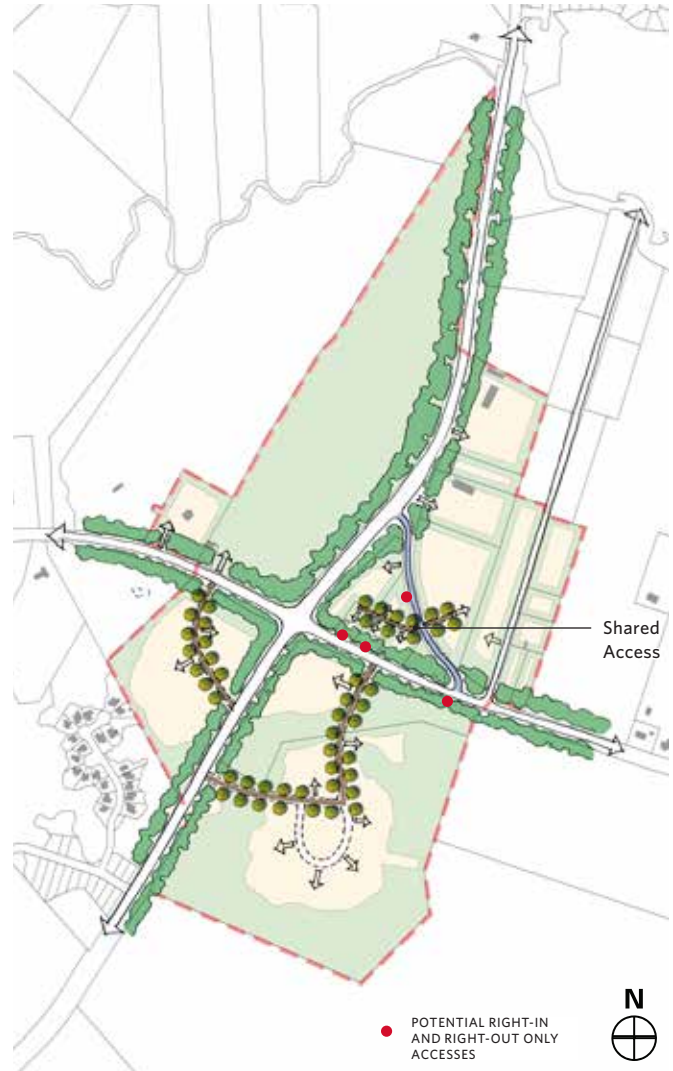
Environmental Constraints and Developable Area

Finally, environmental constraints, including wetlands must be identified through a combination of Army Corps of Engineer mapping and detailed survey work on each site. The developable area is the resulting land after each of these constraints has been subtracted.

DEVELOPMENT AND ACCESS STRATEGIES FOR THE GATEWAY

Based on the redevelopment potential, the County will take one of three approaches to access. Greater levels of parcel consolidation and coordination result in more cohesive development and preserve the historic and visual character of SC 165 and SC 61.

How will this gateway vision be achieved? It will be achieved by the partnership between the County and developers. It is the responsibility of the County, citizens, and property owners to plan for the ideal access/development strategies, which will determine the character of the gateway for the future, by setting the amount of landscape buffer that can be preserved.



Strategy 1: Existing Configuration No parcel consolidation; arterial system remains as existing

Strategy 1: Existing Configuration

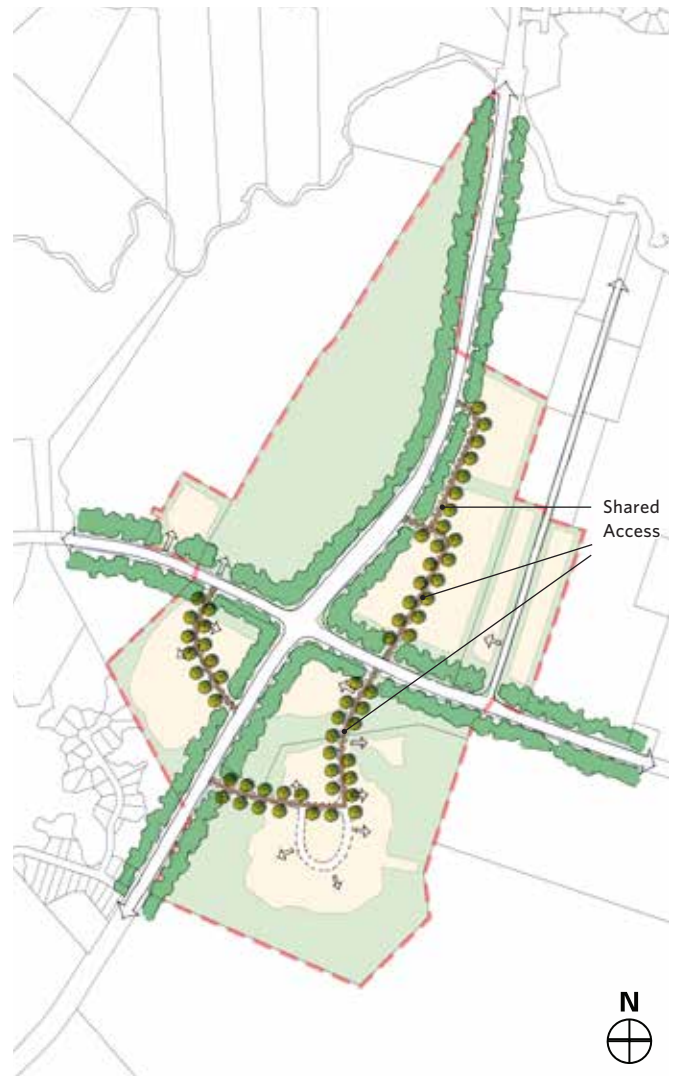
In Strategy 1, the County would leave the arterial system as is and parcels would redevelop individually. This entails the least cost, but has the biggest potential negative impact on the historic and scenic corridors. Where possible, such as on the southeast corner and within the triangle, shared access drives would be required. Where not possible, access would be restricted to right-in and right-out. Most parcels would continue to use existing access, resulting in impacts on buffers and landscape. With limited opportunities for consolidation, this option is the least conducive to creating a welcoming gateway.



Strategy 2: Required Shared Drives Limited parcel consolidation; arterial system remains as existing

Strategy 2: Coordinated Shared Drives

In Strategy 2, the County would incentivize coordinated development by partnering to create a shared drive, on the northeast and southeast corners. The parcels along those drives would be provided access from the shared drives, rather than from SC 165 or SC 61. Strategy 2 requires the consolidation of the parcels within the triangle. This strategy begins to achieve a gateway, by preventing curb cuts on SC 165 and SC 61 and by allowing buildings to be organized around shared spaces, typical of the rural Lowcountry.



Strategy 3: Interconnected Network Maximum parcel consolidation; elimination of the SC 165 'jug handle'

Strategy 3: Interconnected Network

Strategy 3 provides the best opportunity to incentivize development that respects the historic and scenic corridor. The County, in partnership with property owners and developers, would remove the SC 165 jug handle; consolidate parcels to create better configured developable land; and create an interconnected network of access drives, meeting at an intersection with SC 61, replacing the previous jug handle intersection. This strategy provides the best opportunity to create a cohesive character for the development and to retain property values over time.

COMMON ADDRESSES AND SPACES IN THE GATEWAY

One of the strongest placemaking tools is the manner in which buildings are organized related to one another and related to streets, lanes, and open space. There is a heritage of building organized around small, identifiable spaces in rural Lowcountry, often at simple rural crossroads. The scale of SC 61 and SC 165 as large and the depth of setbacks and landscape buffers are not conducive to creating small scaled places. For that reason, the Cooks Crossroads gateway will consist of buildings organized around secondary, smaller addresses within each of the four quadrants. The protection of a green, welcoming gateway and the creation of pedestrian-scale places within the quadrants will add value over time, attracting people and customers to places different from everywhere else that create a competitive advantage

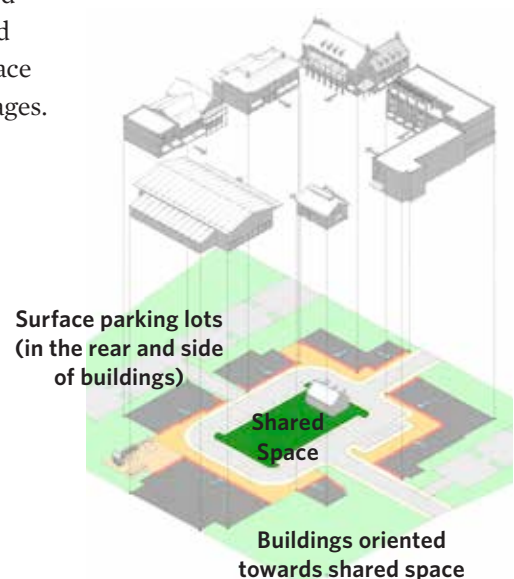
Building orientation will be internal to each site, rather than oriented towards SC 61 and SC 165. This different than conventional development and even some of the more recent development in the County, such as Summers Corner. However, because of the National Scenic Corridor context, the appropriate design approach is different for Cooks Crossroads. Common addresses and spaces achieve the goal of the gateway by preserving tree canopy along the corridors and creating commercially viable spaces internally where people want to be.

Where rears of buildings are visible from public rights-of-way on SC 165 and SC 61, additional screening must be provided to screen views of service functions and buildings must be treated architecturally as fronts.

All new development in Cooks Crossroads will be organized around shared spaces. General requirements for creating common addresses can be found to the right. Every portion of development must be assigned a common space type. Specific requirements for each type can be found on the following pages.

REQUIREMENTS FOR CREATING A SHARED SPACE

- Buildings orient toward shared space.
- Drive aisles must be designed as internal streets.
- Sidewalks must be wide and pedestrian-friendly.
- On-street parking must be provided.
- Signage should be welcoming and well-designed.
- Where appropriate, high-quality fencing should define the edges of property and spaces.
- Elements on the shared space must be pedestrian-scaled.
- An appropriate amount of lighting must be provided to create a comfortable pedestrian environment, while not over-lighting.
- Minimize surface parking area by providing on-street parking where possible and breaking up large parking lots with landscape and pedestrian paths



Plan corresponding to Access Strategy 3, with Shared Space Types identified



Village Lane



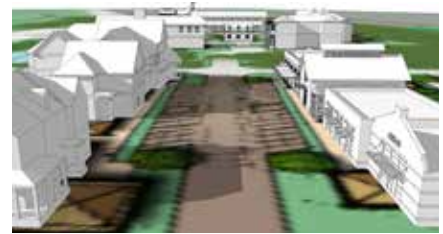
Retail Court



Individual Parcel



Main Street



Retail Cluster



Conservation Neighborhood



GATEWAY PATTERNS

Plan corresponding to Access Strategy 3, with Shared Space Types identified

INFILL DEVELOPMENTS ON EXISTING LOTS

Any new use on existing lots will be required to have shared access and will not be permitted individual curb cuts along SC 61 and SC 165.

Some of the existing parcels in Cooks Crossroads might be limited to small developable area if applying the current zoning requirements with buffer requirements. Several smaller parcels on site might already have structures that are non-conforming to the buffer and setback requirements. Buildings with good conditions or historic values can be redeveloped following the guidelines outlined in zoning *Article XI Zoning and Development Rural District Regulations, Section 11.2.10 Applicability to Existing Non-Conforming Development within the Transitional Overlay District (TOD)*. Reference Page 15 for possible location for this type of development.

CHARACTERISTICS

- Smaller parcels that are constrained by setbacks and buffer requirements
- Limited developable area
- Might need to share access with adjacent parcels
- Avoid having more than one curb cut from SC 61 and SC 165
- Follow agrarian architecture style



Example of infill developments
Buildings are suggestive.



Preserving Natural Features

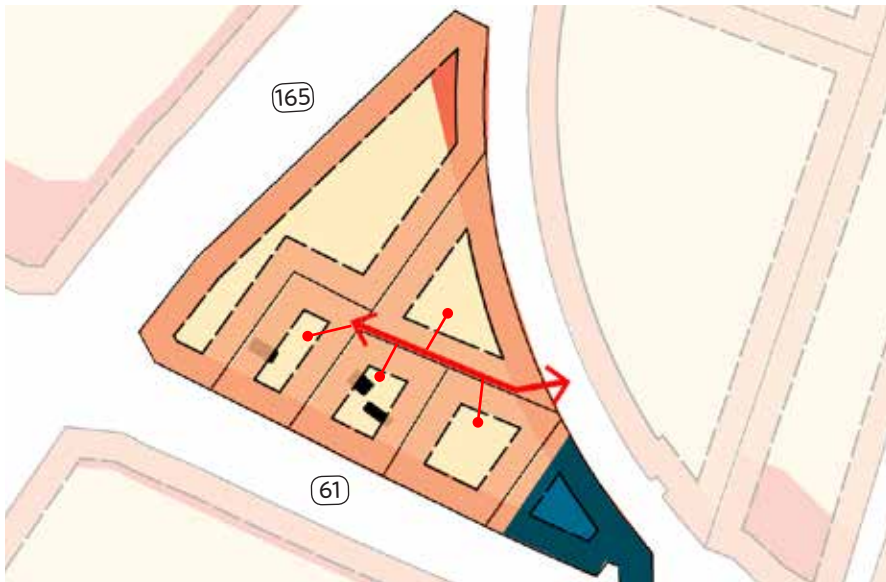
Minimize site disturbance. Any new developments should follow the setback and buffer requirements.





- DEVELOPABLE AREA
- SETBACK
- ASHLEY RIVER HISTORIC OVERLAY DISTRICT BUFFER
- STORMWATER AREA

Constraints
 Limited developable area caused by setback and buffer requirements on existing parcels.



- ↔ SHARED ACCESS WAY
- ACCESS TO INDIVIDUAL PARCELS FROM THE SHARED ACCESS WAY

Shared Access Ways
 Full access drive ways or alleys that serve multiple parcels with minimal curb cut from main roads.



Agrarian Architecture Style
 New developments should use the local agrarian architecture style to match the surrounding context.

VILLAGE LANE

The existing conditions and constraints leave some of the existing properties with long and narrow developable areas, which in several cases have large buffer requirements along Highway-165 and Ashley River Road. The Village Lane spatial type is ideal for site conditions with limited buildable area facing a major arterial. Creating a smaller thoroughfare allows buildings to address onto the lane with slower traffic and easy accessibility to small parking areas in between buildings. Reference Page 15 for possible location for parcels that are suitable to be developed along a village lane. Reference page 17 for suggestion about where in Cooks Crossroad the Village Lane may be appropriate.

CHARACTERISTICS

- Ideal spatial type for portions or parcels with narrow developable area
- Buildings face outward towards the landscape buffers along SC 165 or SC 61
- Buildings front on a lane (one-sided development)
- Parking areas occur between buildings or in the rear

SITE LAYOUT REQUIREMENTS

1. Shared parking areas located in between buildings
2. Building setback from Local Road
3. One major access from Major Arterial that serves all businesses
4. Pedestrian access connecting to regional trail and all businesses
5. Preserved natural buffer area



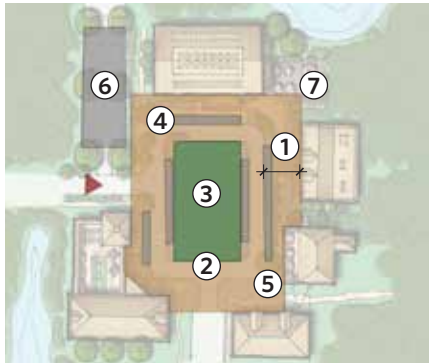
Example of developments along a Village Lane
Buildings are suggestive.



Agrarian Architecture Style
Developments using local agrarian architectural forms to celebrate the rural character.

RETAIL COURT

The Retail Court spatial type works best on parcels where multiple buildings are planned. This type of space allows the creation of a small internal mixed-use district, with a formalized open space in the center with buildings facing onto the space. A small street should serve all of the buildings with on-street parking spaces. The open space in the center should be carefully designed so people feel intimate within it and well connected visually and physically to all the businesses around. Reference Page 15 for possible location for creating a retail court. Reference page 17 for suggestion about where in Cooks Crossroad the Retail Court may be appropriate.



SITE LAYOUT REQUIREMENTS

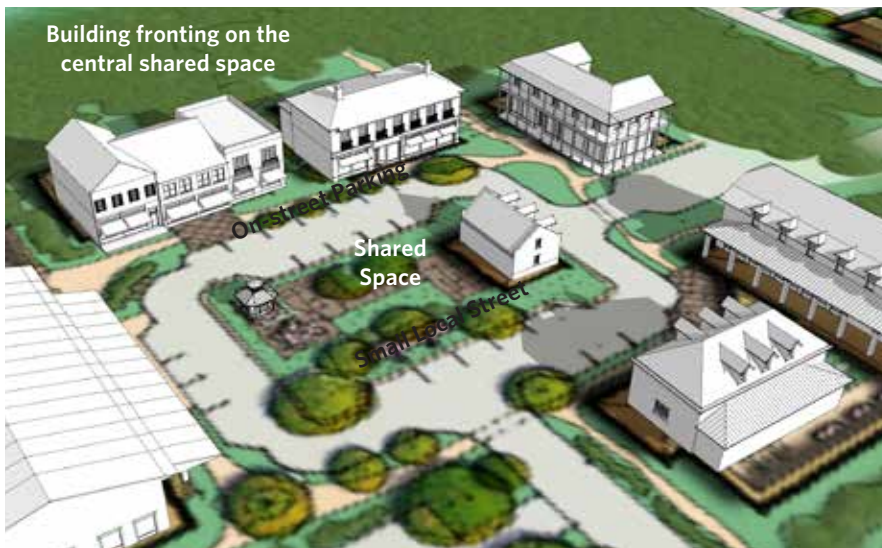
1. Building setback from local street
2. Narrow small local street that serves all businesses
3. Central shared open space
4. On-street parking along local street for convenience parking in front of stores
5. Pedestrian pathway that connects to all building entrances
6. Surface parking lots to be located on the side or rear of buildings
7. Outdoor dining/seating area

CHARACTERISTICS

- Buildings face inward around a formalized space
- Buildings have a more formal relationship to one another (similar setbacks and alignments)
- A small street defines the space
- The gathering space may be a hard-scaped plaza or green and landscaped



GATEWAY PATTERNS



Example of a Retail Court

Buildings are suggestive of possible Lowcountry designs.



Outdoor Living Room

Street furniture, paving materials, and landscape for the shared space and small street should accommodate and encourage pedestrian outdoor activities.

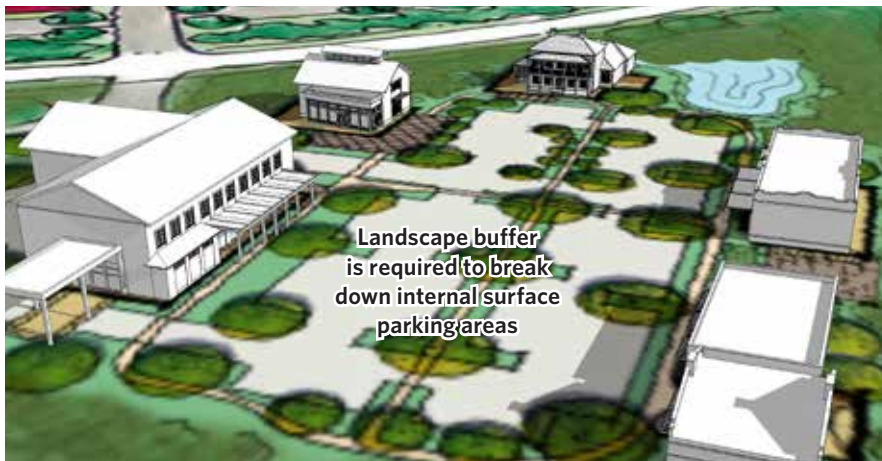
RETAIL CLUSTER

The Retail Clusters are defined by a minimum of four buildings, organized around a small, pedestrian-friendly parking court. Retail cluster spaces have the benefit of being able to be developed on irregular and various sized sites. Individual buildings function as independent businesses. Pedestrian connectivity is required between all buildings. Patios and plazas in the front and to the side of buildings should connect the cluster together through a walkable network. Small gathering areas such as these may incorporate gravel or other informal, rural materials. Parking lots should be well designed and incorporate landscaping features. Reference page 17 for suggestion about where in Cooks Crossroad the Retail Cluster may be appropriate.



SITE LAYOUT REQUIREMENTS

1. One major access from main road that serves the whole retail cluster
2. Minimum 10-foot wide landscape buffer between parking areas
3. Outdoor dining/seating area
4. Well connected pedestrian network through parking areas and serves all building entrances
5. Maximum one bay of parking width (60-64 feet)
6. Locate Service and loading area in the back of buildings
7. Building setback from shared parking area



Example of a Retail Cluster
Buildings are suggestive.

CHARACTERISTICS

- Buildings facing inward towards pedestrian-friendly parking courts
- Informal relationships between the buildings
- Minimum of four buildings required to create a retail court
- Parking should be 120 feet or less in width



Informal Building Relationships
Informal arrangements of buildings are appropriate for the rural context of Cooks Crossroads.

MAIN STREET

The Main Street spatial type is the most straightforward way to create a pedestrian friendly environment for people to enjoy shopping and dining experiences. A small street is designed to slow vehicular traffic and provides on-street parking along both sides. Buildings frame the space on both sides with varied storefronts. Building elements like awnings or arcades should be used to create shade for pedestrians. Outdoor dining or display areas are encouraged to activate the street. Reference page 17 for suggestion about where in Cooks Crossroad Main Street spaces may be appropriate.

CHARACTERISTICS

- Buildings face one other along a small, pedestrian-scaled street
- Buildings are sited with similar setbacks and alignments
- Pedestrians can easily cross from one side of the street to the other
- The Main Street is fronted on both sides by buildings



SITE LAYOUT REQUIREMENTS

1. Surface parking lots to be located on the side or behind buildings
2. On-street parking along Main Street for convenience parking in front of stores
3. Building setback from Main Street
4. Adequate width of pedestrian realm on both sides of the Main Street



Example of a Main Street development
Buildings are suggestive.



Active Storefronts
Well-designed storefronts to activate Main Street spaces with pedestrian scale street furniture and wide sidewalks.

RESIDENTIAL DEVELOPMENT IN THE GATEWAY

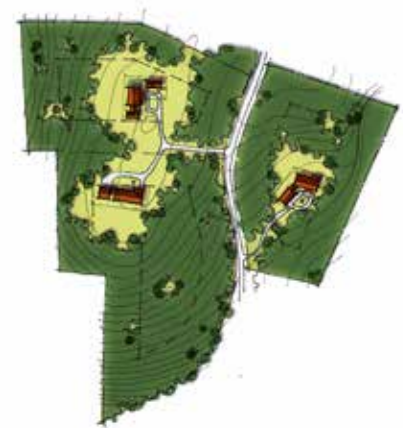
The current ordinance allows for residential on any of the parcels in District 5 at a maximum gross density of 1 unit per 4 acres, a minimum lot width of 100 feet, and a minimum lot area of 1 acre. If residential is developed, these pages provide guidelines and strategies on how best to design for residential living in the Gateway. Conservation development entails siting structures and access in a way that is sensitive to the environment. Houses should be set back from a rural lane, with drives carefully cut through woodlands and around natural features. Clearing of existing trees should be limited to the area around the house and a narrow area for the drive. The natural landscapes are rich with wetlands and woodlands. Houses should be nestled within that context, with natural landscape, and limited or no lawn or suburban yards. Wetland woodland plant species and raingardens are encouraged. On page 17, Residential Development is shown on the southeastern most corner of the Crossroads. Accessory dwelling units above garages may provide for affordable housing options overseen by home owner-occupied residences.

CHARACTERISTICS

- Buildings face one other along a small, pedestrian-scaled street
- Buildings are sited with similar setbacks and alignments
- Pedestrians can easily cross from one side of the street to the other
- The Main Street is fronted on both sides by buildings
- Accessory dwelling units like carriage units are allowed



Example of residential developments surrounding a neighborhood common space
Buildings are suggestive.



Neighborhood Common Space
Houses are accessed by rural lanes. In places, lanes may create central shared green spaces for residents.



Limited Tree Clearing

Reasonably scaled houses can be sited on lots, creating a small clearing for the house and immediate surround, but preserving the majority of the existing trees and vegetation on the lot.



Woodland Setting

Conservation neighborhoods of Cooks Crossing will be developed at low densities, with houses carefully sited to fit within woodlands and wetlands.



Creating Gateways

Entries to neighborhoods and spaces are marked by the placement of small accessory buildings, such as gatehouses, carriage houses, and pavilions.

MULTI-MODAL ACCESS

New development must contribute to the overall connectivity of Cooks Crossroads. This ensures access to neighboring properties, reduces the number of curb cuts and interruptions in the visual buffers, and helps to preserve and protect the character of the historic and scenic corridors.

The Access Strategies on Page 12-13 provide examples of how the streets could be laid out based on the parcel consolidation. If existing roadways are kept as they are, there will be shared private access between parcels that have limited access to SC 61 and SC 165. If parcel consolidation occurs, public internal street networks are encouraged to serve all the developments.

Future street networks may not match the exact alignments or street types shown in the diagram below. However, the principle of connectivity, creating a new intersection with Ashley River Road east of Highway 165, and selecting street designs from the types on the following pages are requirements for vehicular access in Cooks Crossroads.

Intersection of SC 165 and SC 61

Enhancements to the intersection and to the SC 165 and SC 61 rights-of-way are critical to the success of a gateway at Cooks Crossroads. A list of recommended improvements can be found in the list to the right. Furthermore, a median in the center of SC 165 would help to soften the appearance and to create the welcoming character leading into the gateway. Both Ashley River Road and SC 165 should prioritize rural, scenic character, traffic calming devices, and strategies for managing stormwater runoff to combat their stark appearances.



REFERENCES

For specific access requirements, refer to:

- [SCDOT ARMS Chapter 3 - Driveways](#)
- [Article XI Special Areas Section 11.2 Transitional Overlay District \(TOD\) Section 11.5 Ashley River Historic Overlay District](#)

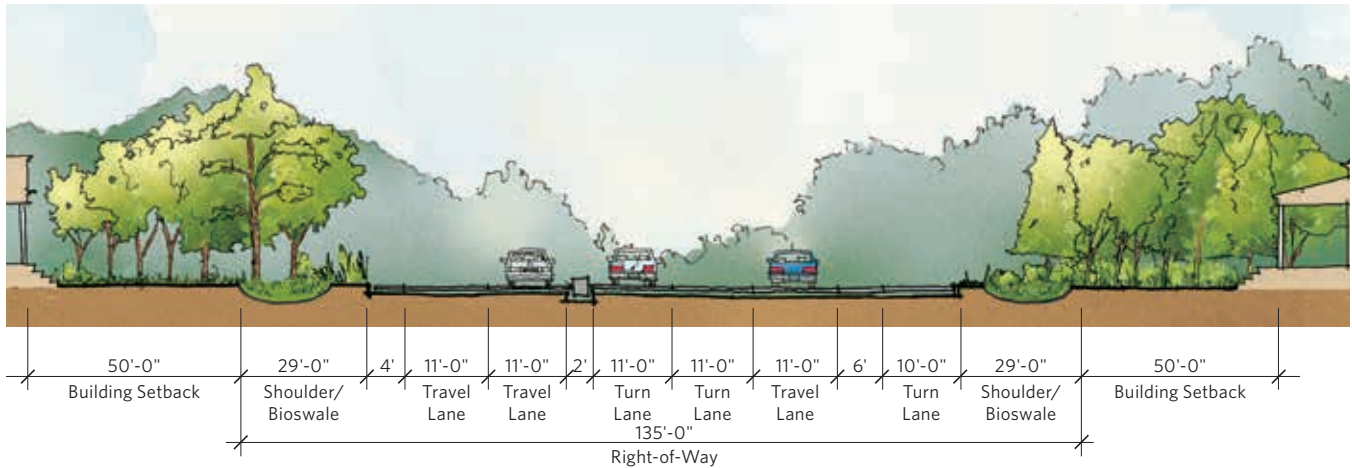
- ASHLEY RIVER ROAD (SC 61) AND HIGHWAY SC 165
- MAIN STREET
- VILLAGE LANE
- CONSERVATION NEIGHBORHOOD LANE

Street Types Key Map - Framework Strategy 3

An internal network of local streets (Main Street, Village Lane, or Conservation Neighborhood Lane) is recommended for Cooks Crossroads. Each private development should establish a well connected road system to serve uses on the site.

STREET TYPES

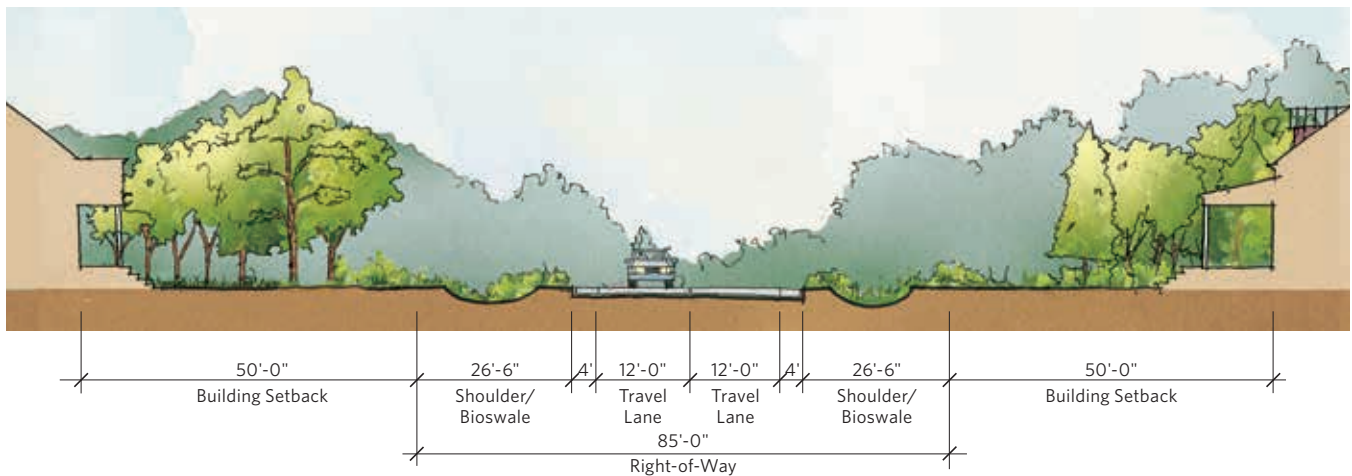
Ashley River Road (SC 61) at Cooks Crossroads Intersection



SC 61 at Cooks Crossroads Intersection

A minimum of 50 feet of building setback is required along both sides of SC 61 (Ashley River Road) to preserve the natural vegetation and view corridors. The State is responsible for maintenance within the right-of-way. Curb cut requirements are outlined in the SCDOT Access & Roadside Management Standards. Improvements at the intersection should prioritize pedestrian crossings.

Typical Ashley River Road (SC 61) Section



Typical SC 61 Section with Improvements

The South Carolina Department of Transportation (SCDOT) has identified SC 61 in Dorchester County for safety improvements through SCDOT's Rural Roads Safety program. The primary focus of this safety initiative is to reduce road departure related crashes that result in fatal and serious injuries. Projects will be developed along Ashley River Road that help to keep vehicles on the road. The design is being developed through a separate process; however, this provides an opportunity to preserve and enhance the rural character of SC 61 in the process.

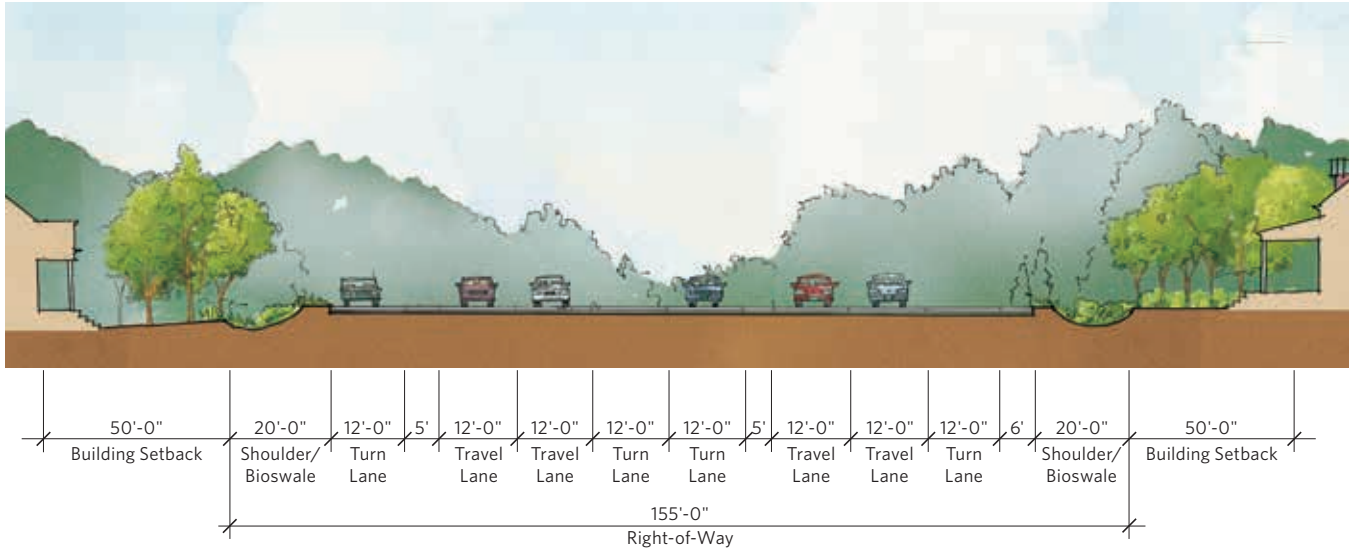
References

- [SCDOT ARMS Chapter 3 - Driveways](#)
- [Article XI. Special Areas](#)
- [Section 11.2 Transitional Overlay District \(TOD\)](#)
- [Section 11.4 Ashley River Corridor Overlay District](#)

POSSIBLE RURAL ROAD SAFETY IMPROVEMENTS FOR SC 61

- Rumble strips and raised pavement markers
- Reflective signs
- Wide pavement markings
- Guardrails
- Wider/paved shoulders and clear zones
- Relocated ditches

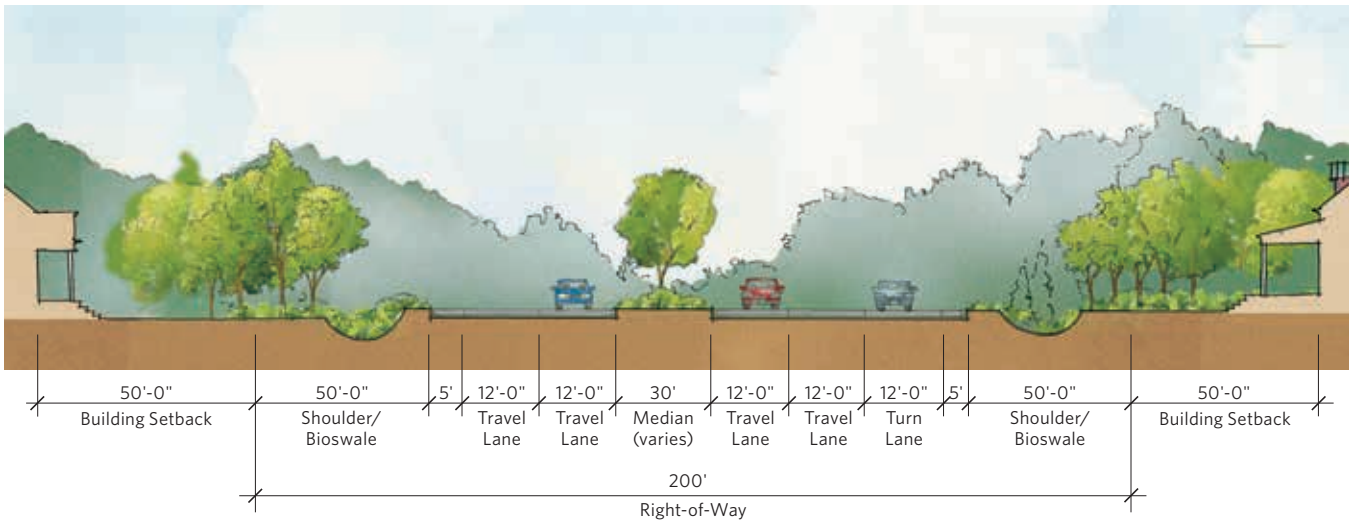
Bacons Bridge Road (SC 165) at Cooks Crossroads Intersection █



SC 165 at Cooks Crossroads Intersection

A minimum of 50 feet of building setback is required along both sides of SC 165 (Bacons Bridge Road). The State is responsible for maintenance of SC 165. Improvements at the intersection should prioritize pedestrian crossings.

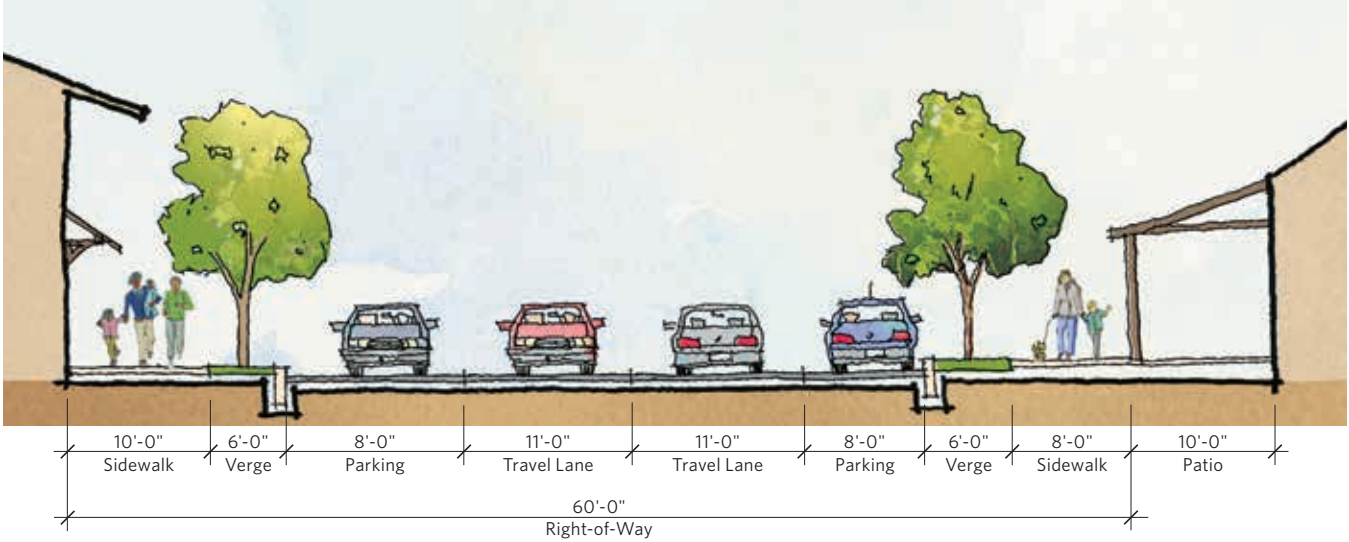
Typical Bacons Bridge Road (SC 165) Section █



Recommended Typical Section for SC 165

A minimum of 50 feet of building setback is required along both sides of SC 61 (Ashley River Road) and SC 165 to preserve the natural vegetation and view corridors. A median between opposite directions of travel lanes is recommended to create the green gateway into the Crossroads.

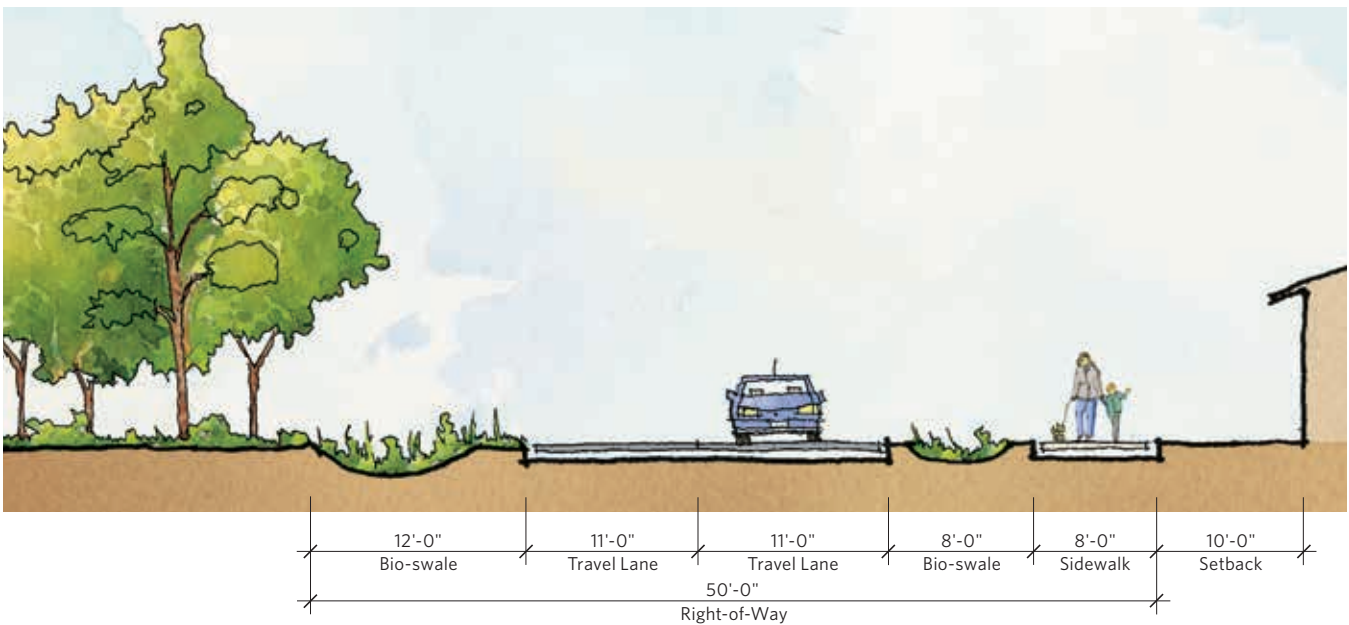
Main Street (Internal)



Local Main Street

Private developments and/or the County will create internal streets that connect various developments to major arterials like SC 61 or SC 165. For any retail Main Street spatial types, it is recommended to have one travel lane each way and on-street parkings along both sides of the road. This parking may be either parallel or head-in diagonal. Retail buildings should sit close to the street, with a wide public realm of 15 to 25 feet from the back of curb.

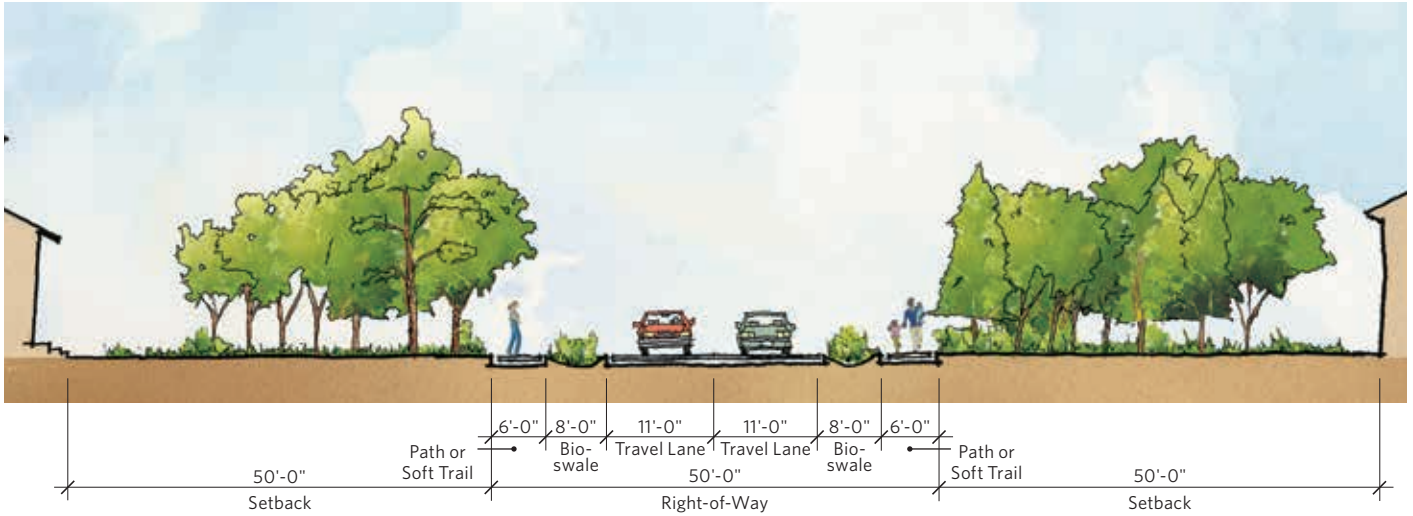
Village Lane/Access Way



Village Lane with One Side Developments

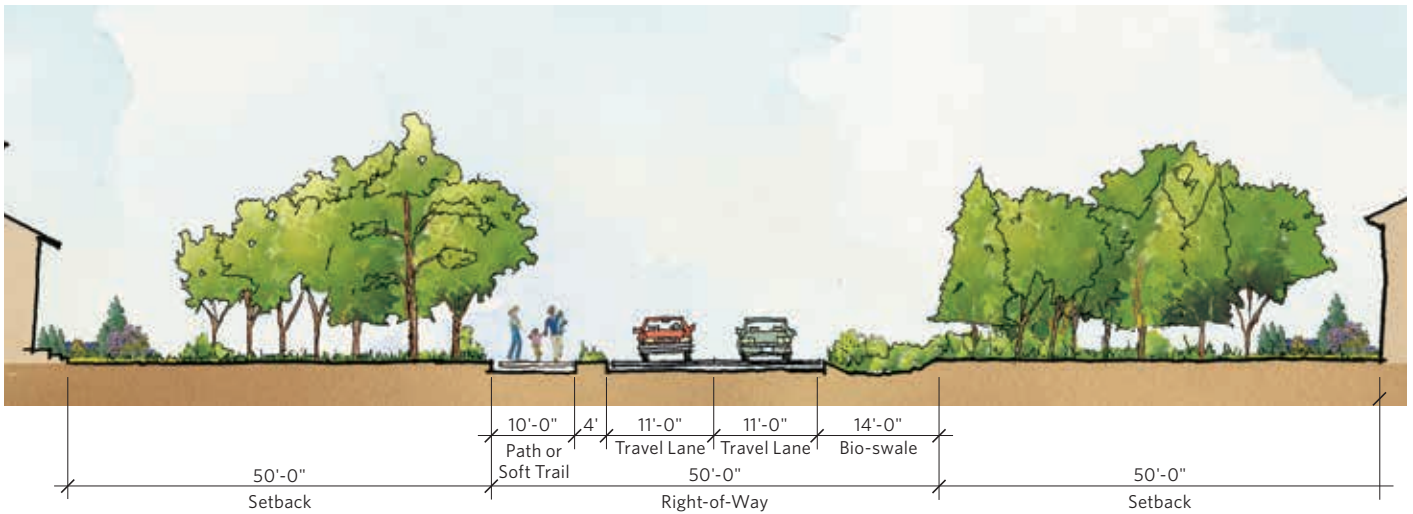
A small street with one travel lane for each direction is recommended for Village Lanes within the development parcels. It should minimize the paved width with natural swales on both sides for stormwater runoff. Developments occurs on one side of the street with the natural elements preserved on the other side. Pedestrian access should be provided on the side of the development, connecting development to regional trail networks.

Conservation Street



Residential Conservation Street

Maximum width of 50 feet ROW street should be designed for residential developments with one travel lane each way and pedestrian pathways on each side. A 50 feet setback is required for any residential buildings. Bioswales are recommended to be designed into the Right-of-Way or within the setback area to slow down surface runoff and remove debris and pollution out of stormwater.



Residential Conservation Street Alternative

Pedestrian on both sides may be replaced with a wider trail on one side of the street.

PEDESTRIAN AND BICYCLE ACCESS

New developments in District 5 of Ashley River Historical District should provide pedestrian and bicycle accesses to the Ashley River Blue Trail and other regional trail networks connecting to nearby neighborhoods such as Downtown Summerville and Summers Corner. The trail map to the right shows recommended trail locations within Cooks Crossroads. Trail segments should be incorporated in the buffer areas within individual site plans so that they link to one another to create continuous trail access. All buildings should have direct pedestrian and bicycle access that connect building entrances to nearby trails. Property owners are responsible to work with the County on detailed trail designs and approvals. Trails should be built outside of rights-of-way and maintained by the property owners. A safe connection across SC 165 at Bacon's Bridge is essential to the interconnected network.

Multi-Use Path Alignment along US-165

The power line easement on the west side parallel to SC 165 is a key opportunity for a pedestrian and bicycle path along Rosebrock Park. Close to the SC 165 jug handle, the easement switches to the east side of the Crossroads. Locating the multi-use path in the easement not only provides new active transportation access, but also provides a paved accessway for utility service.



(Above) Proposed trails through Cooks Crossroads, connecting to the regional trail system opportunities

(Left) Cooks Crossroads will provide a destination for cyclists to stop and enjoy food or beverage.

(Below) Cooks Crossroads and regional trail network.



PUBLIC REALM ELEMENTS

FENCES AND SITE WALLS

Fencing

Four-board wood fencing should be a common identifier for the Cooks Crossroads development. Fences are most appropriate at the edges of the four quadrants, but there may be limited rural fencing used within shared spaces. The fences should be uniform in appearance, with a standard, black finish and four evenly spaced rails. The ends of the rails should rest within the vertical posts. Animal wire is optional on the back side of the fence with or without landscaping.

Site Walls

Low brick or stone walls are typical in the region and may be used to define edges of public space. Low site walls are most appropriate within shared spaces.

Reference:

[13.2.7 Fence and Wall Standards](#)

[13.2.5 Privacy Fencing](#)

LIGHTING

Special attention should be given to control light pollution. Full cut-off light fixtures should be used for street lights and other down lighting fixtures to encourage dark sky features. Up-lighting should be limited to one up-lit sign per quadrant of Cook's Crossroads; the northeast southeast, southwest, and northwest corners.

Commercial developments must limit light levels to minimize pollution, protect investment in public parks, and protect bird and wildlife habitats, as many bird populations migrate along river corridors, including the Ashley River, at night.

Reference:

[Article XIII. Site Development Standards 13.1.3 Site Lighting Standards](#)



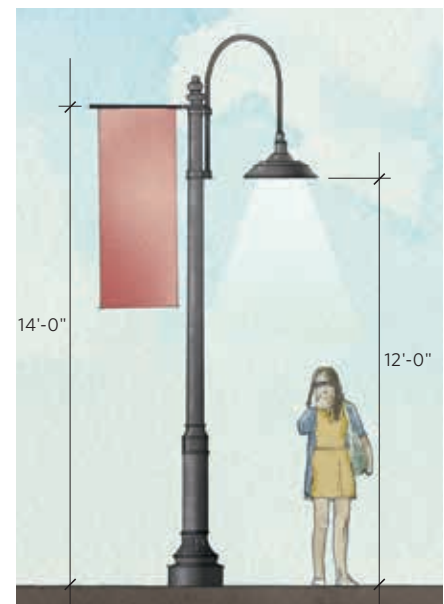
Examples of a pedestrian-scale street lighting



Low Walls Low stone or brick walls are typical in the region and should be used to define edges of public space.



4-Post Fence Black 4-post fences should be used in key locations to designate the edge of retail developments.



Pedestrian-Scale Lighting 14-foot lamp posts, with historical character, shall be selected for development within Cooks Crossroads.

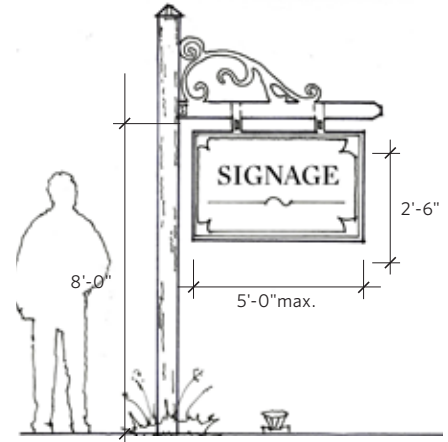
COMMUNITY SIGNAGE

Sign Regulations

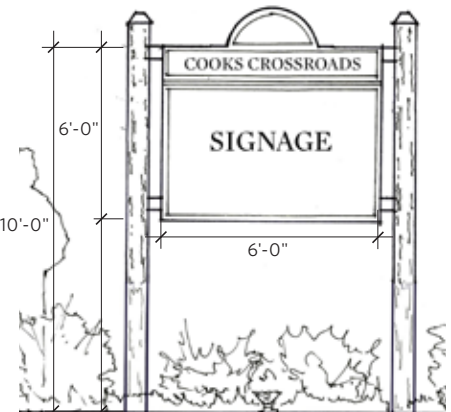
Any signs erected must conform to the standards set forth in [Article XIII, Section 13.5](#) and [Article XI, Section 11.2.8 \(D\)](#) and should be environmentally sensitive to the area.

- Identity signage
 - Development identity signage should be used to identify a particular area of Cook's Crossroads; the northeast corner, the southeast corner, the southwest corner, and the northwest corner. Only one development sign should be used per corner and should include information about the uses within that area.
 - Internal way finding signs should be clear and in a style consistent with the buildings.

This approach of limiting community signage to one sign per quadrant prioritizes design and eliminates visual clutter, maintaining high values. Cooks Crossroads will be a singular destination that people refer to (i.e. 'at the Crossroads'). Furthermore, the way that people make decisions about commercial destinations is changing. The prominence of mobile map searching is decreasing the number of impulse decisions made based on roadside signage.



Example of building identity signage



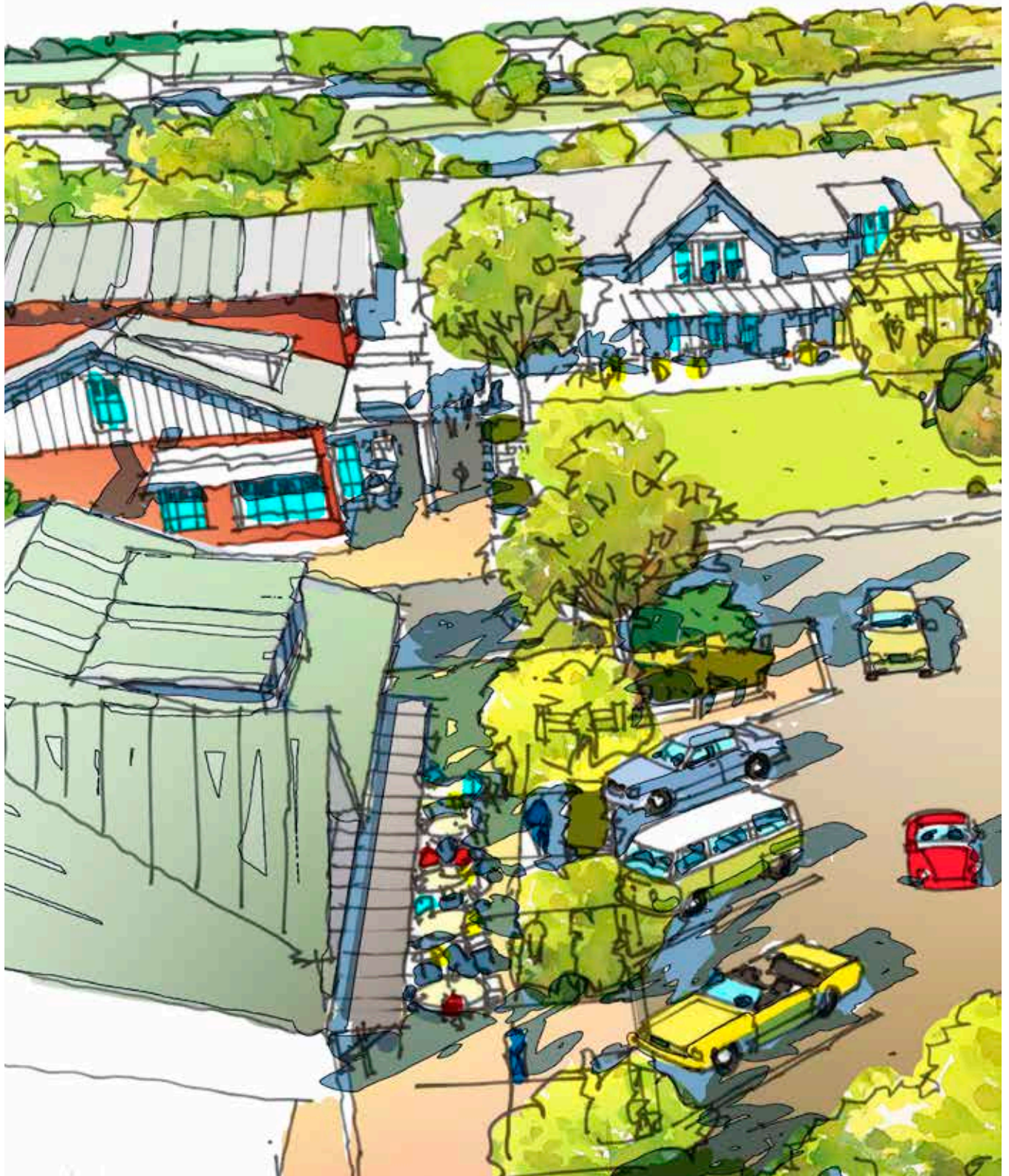
Example of Development Identity Signage, which should have a cohesive aesthetic and should be coordinated between sites and developers.

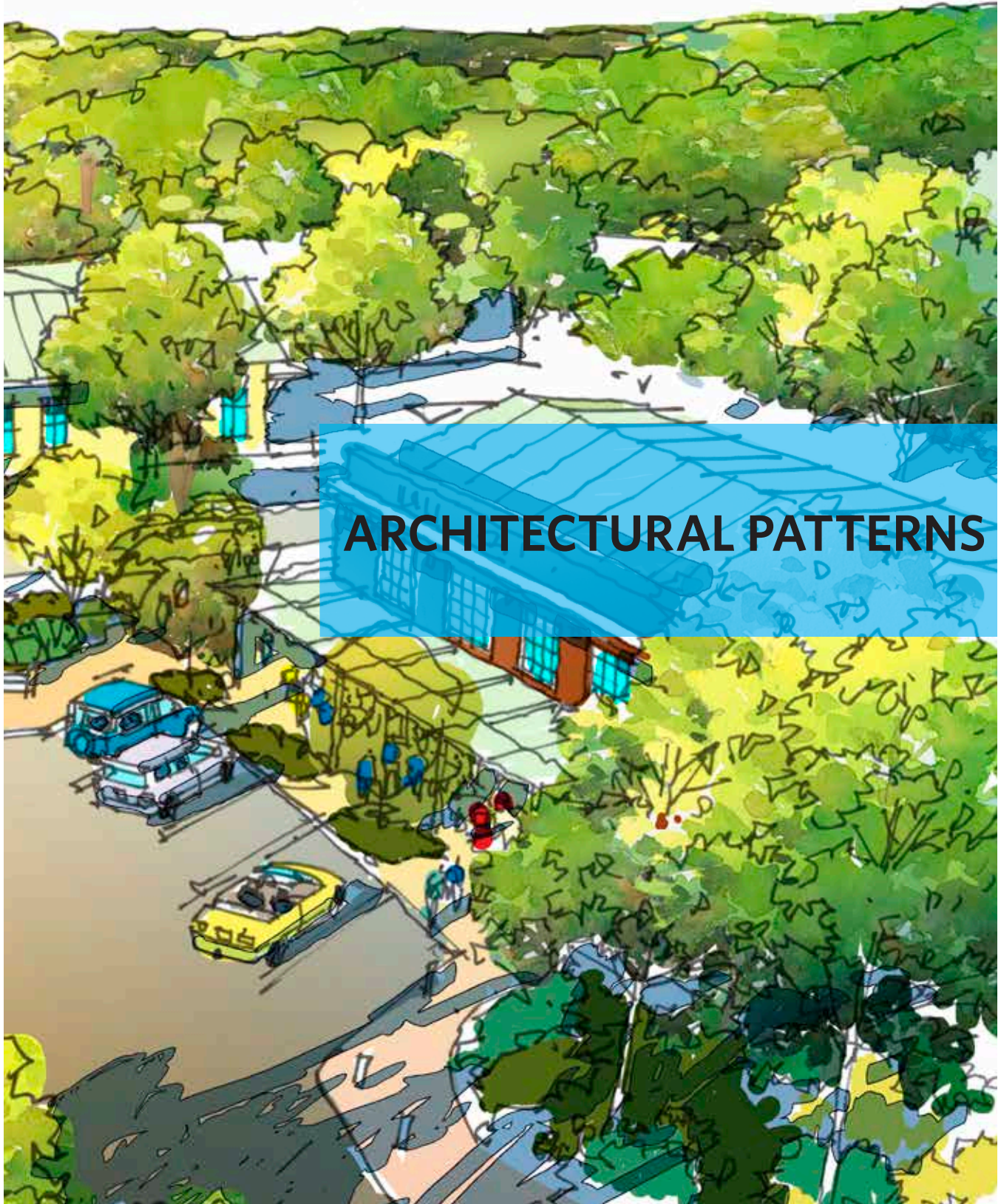


Community Signage A simple sign may hang on character fencing to communicate the village's identity. Signs must comply with the ordinances and should be in keeping with the character of the Crossroads.



Examples of community signage





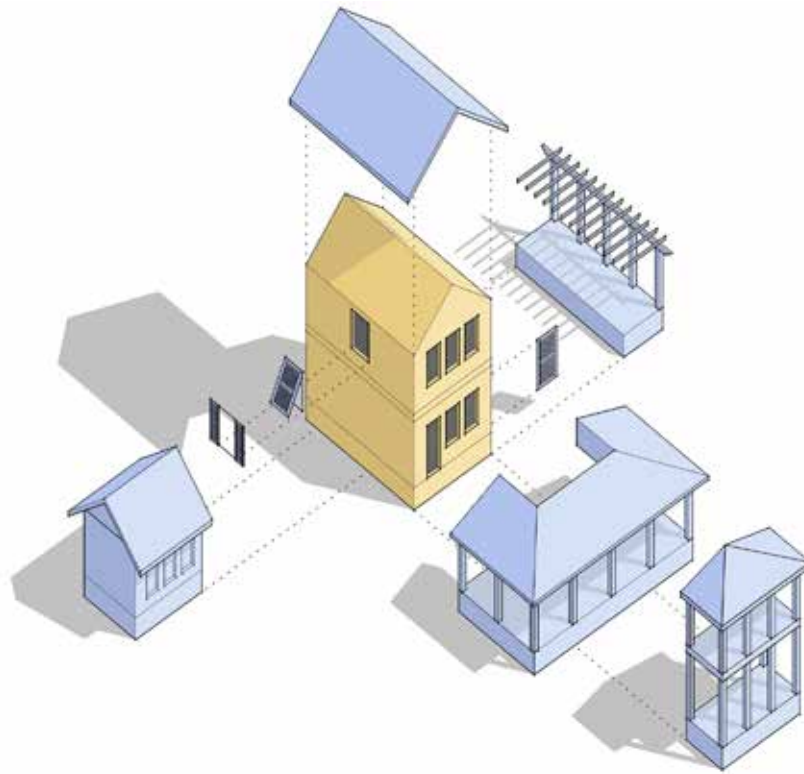
ARCHITECTURAL PATTERNS

ARCHITECTURAL DESIGN STRATEGY

Buildings within the Cooks Crossroads sites should be designed in the tradition of Lowcountry architecture. While there are many different expressions of character, generally these building types evolved from early Classical and Colonial detailing with broad living porches and tall windows and doors with operable shutters for weather protection and ventilation. Buildings were typically elevated 2 to 4 feet above the ground to avoid excessive exposure to ground moisture and to accommodate the seasonal rains. Commercial buildings may be constructed as simple, purpose-built structures with lower ground floors to facilitate public access.

Larger floor plate buildings over 3,000 square feet in footprint, can be designed to reflect a series of smaller, attached buildings that form a village main street or court form. Commercial Buildings 15,000 square feet and above should be designed to reflect traditional agrarian buildings such as stables, barns and traditional warehouses or mills, with a variety of roof forms and massing that breaks down into smaller components. Hospitality buildings should be designed in the tradition of regional Lowcountry Inns and hotels up to 2.5 stories height.





Residential building type with commercial use



Small-scale commercial building precedent



Mid-size commercial building precedent



Large footprint commercial building precedent



Hospitality building precedent

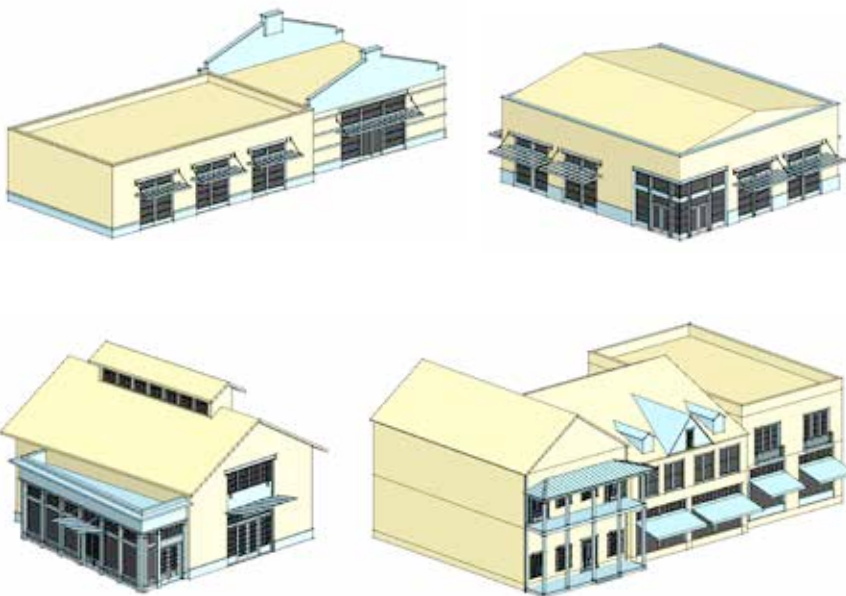


Accessory building precedent

VILLAGE COMMERCIAL BUILDINGS

Large format commercial buildings must be designed as a series of different size and shape forms that read as smaller, more human scale building forms that are joined together to create the large format interior spaces required by the program. Uses such as grocers and pharmacies or specialty building material suppliers and cinemas are examples of these types of buildings. The goal is to design in such a way that these larger buildings can fit into a village scale context within the Cooks Crossroads gateway. The generic qualities of a “big box” commercial building are out of character with the smaller, more articulated buildings that formed most Lowcountry settlements outside of industrial and warehouse districts.

The guidelines call for the careful massing of these buildings to achieve the desired fit into the Crossroads development patterns and relate to the more agrarian building types found throughout the region such as traditional stables, barns, and storage buildings.



BUILDING PLACEMENT

- Building Setback from SC 61 and SC 165
50 feet or applied buffer,
whichever one is greater
- Distance between main building to back
of Sidewalks, Street, Court, or Common
Parking Court 0-10 feet
- Distance between buildings min. 0 feet
- Distance between the rear of buildings
and sidewalk or parking lot min. 5 feet

FOOTPRINT

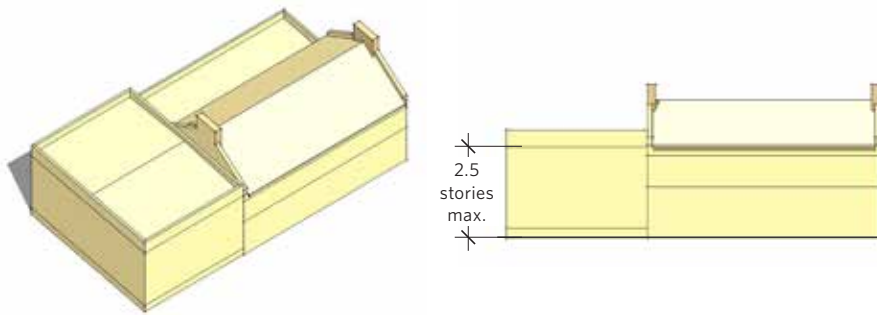
- Stand alone 3000 sq. ft. max
- Attached 15,000 sq. ft. max

HEIGHT GUIDELINES

- Overall Height (Stories) 2.5 max
- Maximum Height
(Principle Building) 35 feet
- Floor-to-Ceiling Height
 - 14-20 feet — Ground Floor
 - 9-14 feet — Second Floor

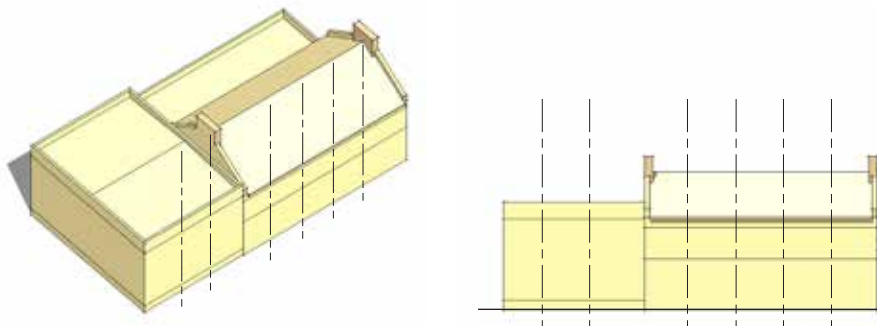
Massing and Height

Mixed-use buildings can be up to 2.5 stories, not to exceed 35 feet. Parapets, towers, architectural elements and other decorative features are permitted above 35 feet. Buildings larger than 3000 square feet can be designed as a series of smaller buildings attached to form a bigger building along the street up to 15,000 square feet in ground floor area.



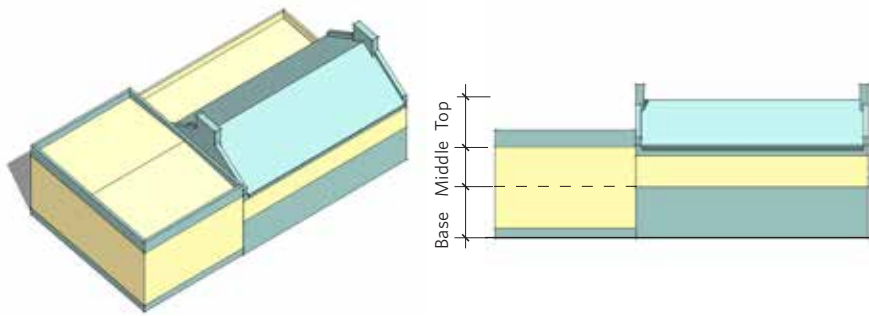
Vertical Articulation

The storefronts on the ground floor should be vertically proportioned into bays corresponding to the rhythm of windows above. Large development blocks should use vertical articulation at intervals no greater than 60 feet to compose the facade as a series of smaller buildings with varying widths. The changes in plane should be significant enough to create a sense of depth and shadow.



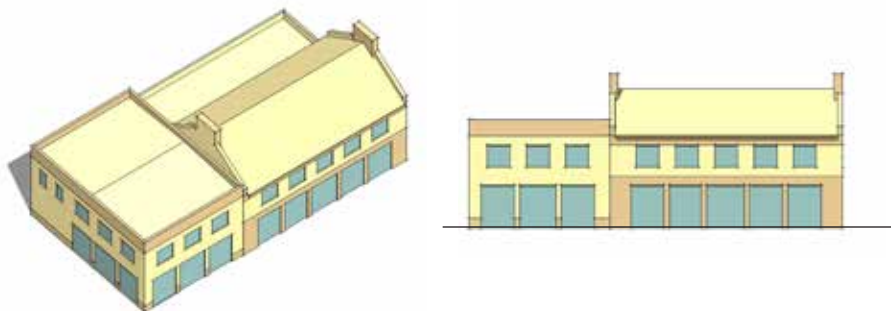
Horizontal Articulation

Buildings should have a recognizable base, middle, and top. Ground floor retail storefronts should sit at grade wherever possible. Varying roof and cornice lines should articulate the top of the building. The block development should read as a series of smaller heritage buildings.



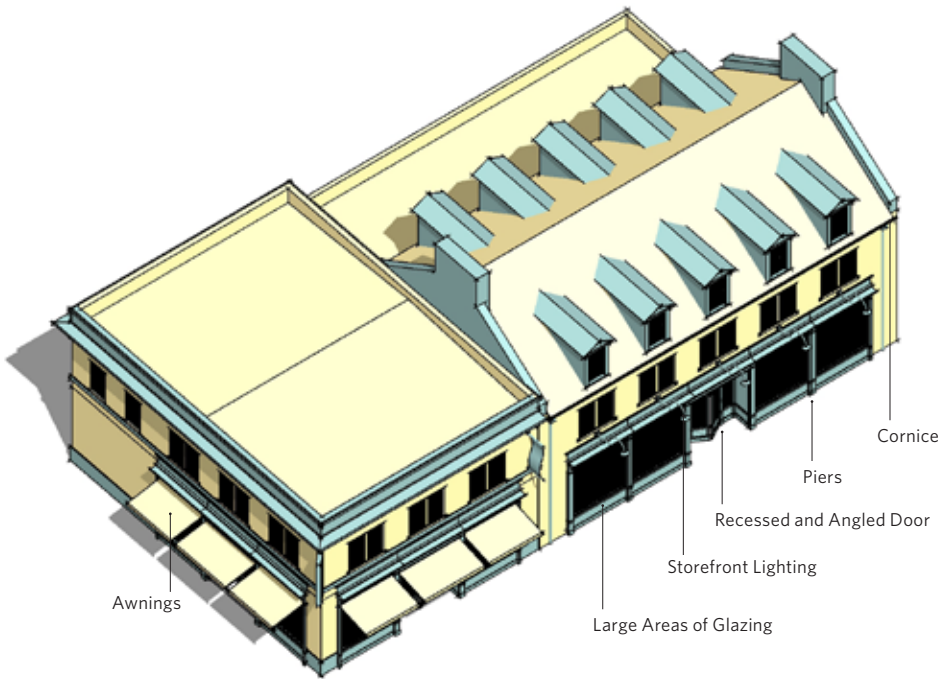
Windows and Doors

Windows occur in patterns of single or grouped double-hung windows that reinforce the vertical articulation of the building mass and bays. Doors are located in appropriate and prominent locations that front public space or the street. Lintels, trim, transoms, and other decorative door and window features are important parts of the pattern and should be designed and sized to match the size of the opening, the mass of the building and the general design vocabulary.



Building Articulation

The main massing (yellow) should be simple geometries with additive elements (blue) to add facade variety and livable space to each unit. Special elements such as bay windows, roof dormers, balconies, decorative eaves and cornices, canopies, awnings, decorative signage and highly articulated storefronts should be provided.



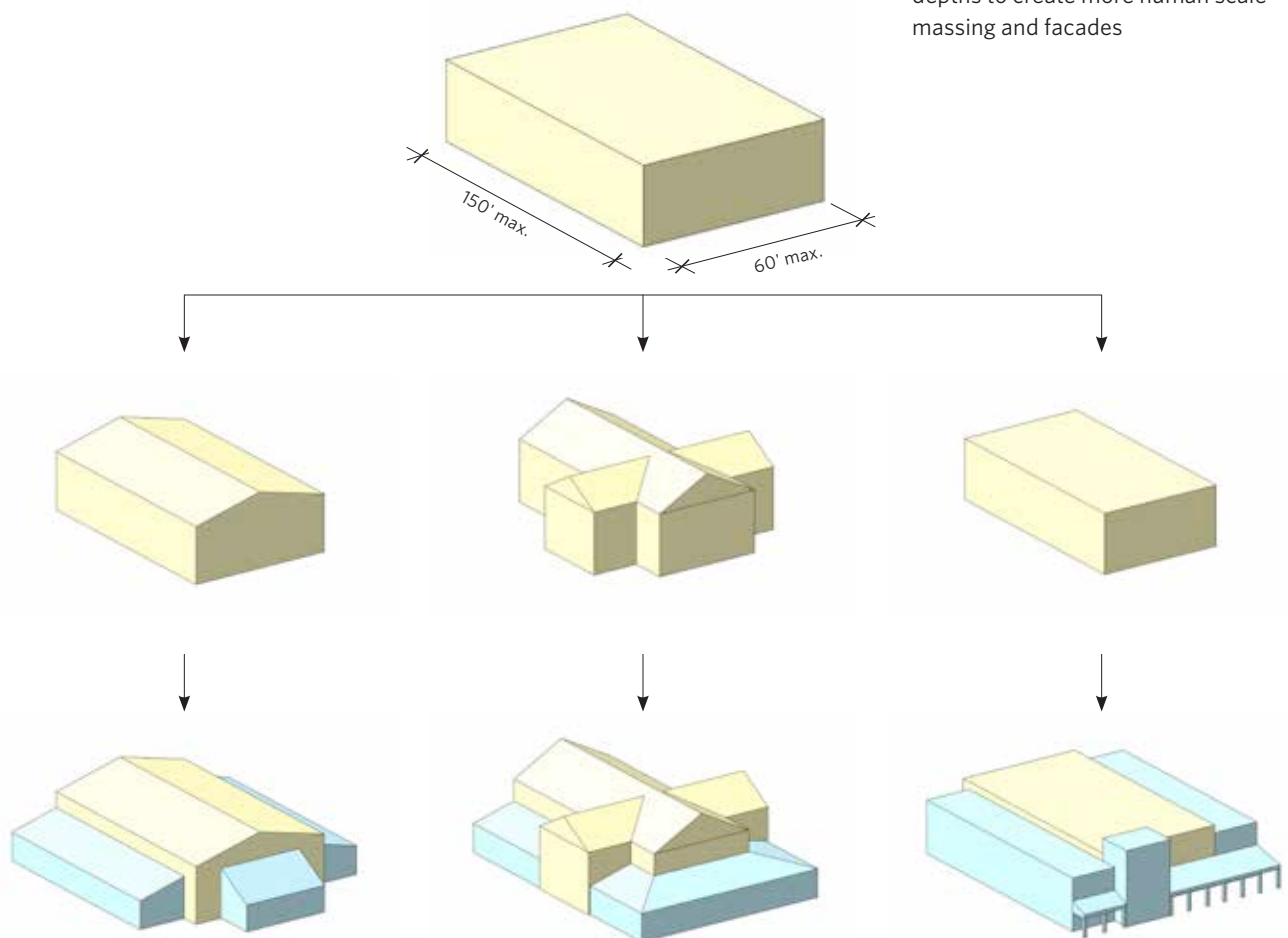
LARGE FORMAT COMMERCIAL BUILDING

Large format commercial buildings must be designed as a series of different size and shape forms that read as smaller, more human scale building forms that are joined together to create the large format interior spaces required by the program. Uses such as grocers and pharmacies or specialty building material suppliers and cinemas are examples of these types of buildings. The goal is to design in such a way that these larger buildings can fit into a village scale context within the Cooks Crossroads gateway. The generic qualities of a “big box” commercial building are out of character with the smaller, more articulated buildings that formed most Lowcountry settlements outside of industrial and warehouse districts.

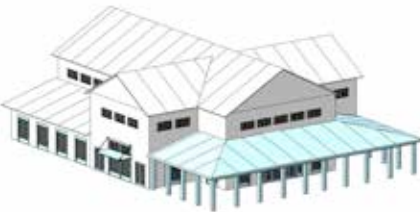
The guidelines call for the careful massing of these buildings to achieve the desired fit into the Crossroads development patterns and relate to the more agrarian building types found throughout the region such as traditional stables, barns, and storage buildings. All large format commercial buildings should be designed in the Lowcountry architectural heritage.

LARGE SCALE COMMERCIAL BUILDING

- Typically houses single uses (grocery, pharmacy, etc.)
- Footprints greater than 15,000 SF
- Siting with hardscape and pedestrian access at primary entrances with
- Significant tree and landscape elements planted close to areas of the building without composed window and door patterns.
- Building form and elements should be agrarian in character
- 2-story, barn or shed massing typical
- Flat-roofed building must be broken down into varied facade widths and depths to create more human scale massing and facades



Large Scale Commercial



HOSPITALITY BUILDINGS

Hospitality buildings contain multiple units consolidated into a single structure. These buildings can be integrated into a village street environment fronting the street or can be set in a landscape at the edge of a commercial village. The building should face either a street or courtyard to create a village-like setting. Parking should be placed behind the building with minimal views to parking from the street or public space. Pedestrian and bike access linking the hospitality use to other amenities and uses accessible from the District is important to creating a cohesive place. Buildings should be of a scale so that no single building dominates the landscape. Hospitality buildings can help to form the welcoming appearance and overall character of the gateway.

BUILDING PLACEMENT

- Building Setback from SC 61 and SC 165
50 feet or applied buffer,
whichever one is greater
- Distance between main building to back
of Sidewalks, Street, Court, or Common
Parking Court 15-30 feet
- Distance between buildings min. 10 feet
- Distance between the rear of buildings
and sidewalk or parking lot min. 5 feet

HEIGHT GUIDELINES

- Overall Height (Stories) 2.5 max.
- Maximum Height
(Principle Building) 35 feet
- Floor-to-Ceiling Height
10-14 feet — Ground Floor
9-14 feet — Second Floor

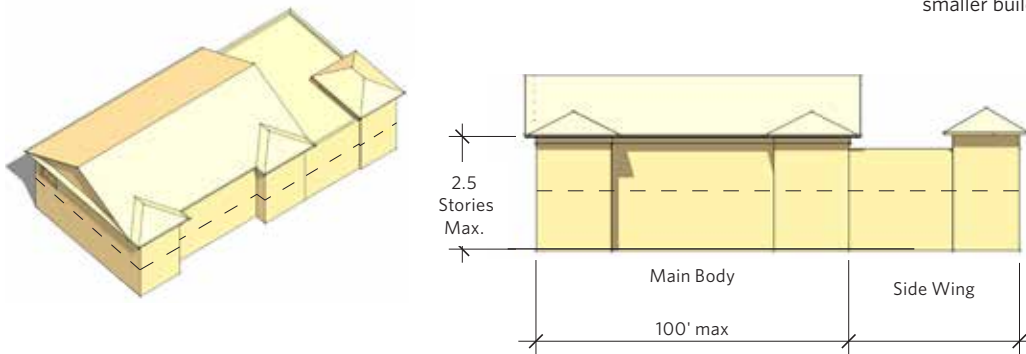


Massing and Height

Hospitality buildings can be a maximum of 2.5 stories. To avoid long monotonous facades, the buildings should be articulated. Typically a central mass or Main Body, with supporting wings that are set back from the Main Body and may step down in height. Maximum length of the Main Body front facade should not exceed 100 feet. Maximum length of front facade is 260 feet with wings. Heights should vary to avoid a continuous block at one height and to simulate a row of historic facades.



Larger buildings can be broken down into smaller building forms

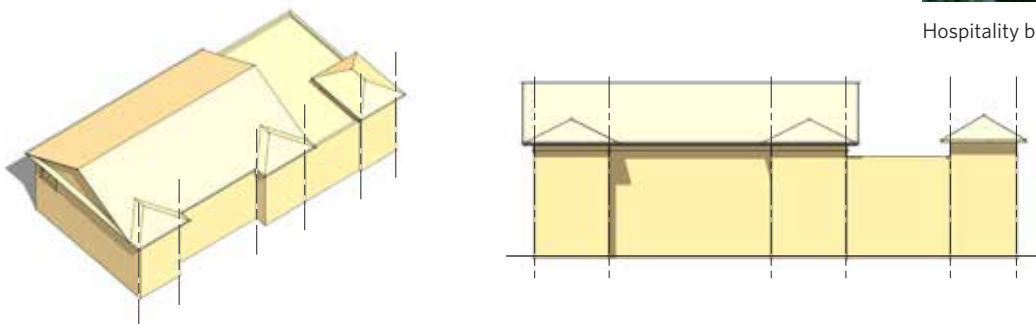


Vertical Articulation

Vertical elements such as piers, pilasters, trim, window mullions, and bay windows should be used to compose the facade. Facades shall be articulated with a vertical change in plane at intervals no greater than 60 feet. The changes in plane should be significant enough to create a sense of depth and shadow.



Hospitality building fronting a village street

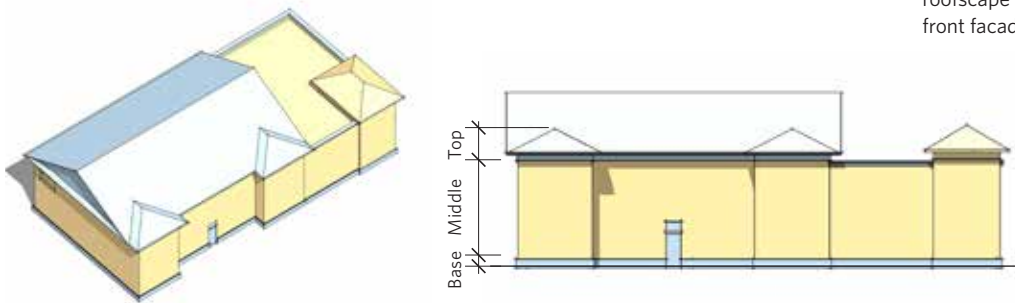


Horizontal Articulation

The base, middle, and top are the three major levels of horizontal articulation. The base sits above grade to provide a strong exterior presence on the street. When elevated a significant amount, it can provide safety and external noise reduction to units that front the street. The middle of the facades is articulated through the window patterns. The roofscape and articulation of the top floor should vary to enrich the facade of the street.



A large hospitality building with articulated roofscape and form to create an interesting front facade facing the street



Windows and Doors

The window pattern and dimensions should work well with the layout of the principal rooms of the hospitality on the interior. The windows should be organized similarly to the vertical articulation to create a consistent bay rhythm in the facades of the building.

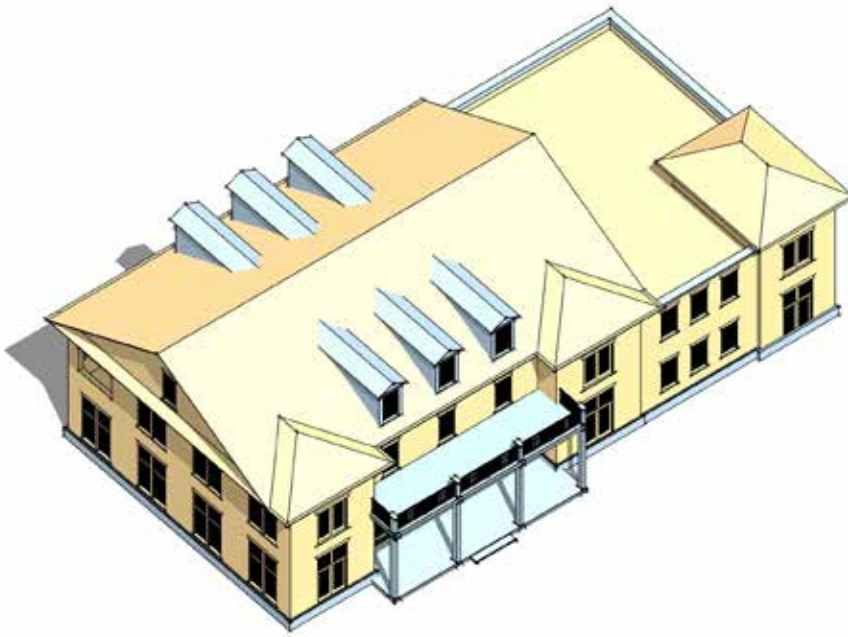


Windows and doors should be organized systematically to create consistent building bays



Building Articulation

The main massing (yellow) should be simple geometries with additive elements (blue) to add facade variety and livable space to each unit. Special elements such as bay windows, roof dormers, balconies, decorative eaves and cornices, and ornamented doorways should be provided.



Porches and balconies with varied roof forms added to the main body of a hospitality building



Hospitality building with building articulations to break down the overall massing



Large front porch and additional living spaces added across the front of the building

RESIDENTIAL BUILDING TYPES

Many commercial buildings were originally designed as residential architecture with adaptations for commercial uses occurring over time. Many towns and commercial districts within the region have a collection of purpose built commercial buildings as well as residential buildings that have changed to commercial uses over time. This creates a distinct character and charm for Lowcountry settlements that continues today. Cooks Crossroads should have a mix of multiple building types and character throughout. In areas of the District that choose to develop as single-family detached conservation lots, these guidelines would also apply.

The architectural vocabulary for these types focuses on traditional Lowcountry design elements from the Colonial and Picturesque patterns found in the region.

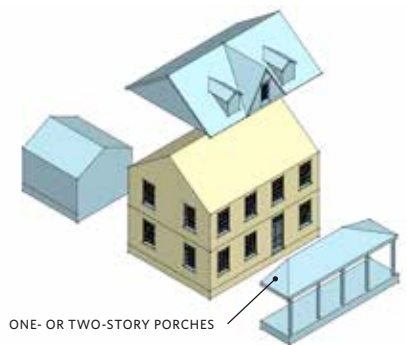


BUILDING PLACEMENT

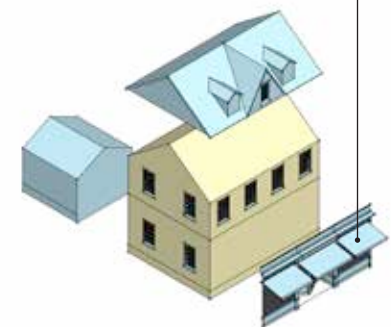
- Building Setback from SC 61 and SC 165
50 feet or applied buffer, whichever one is greater
- Front Setback from Internal Street 10-30 feet
- Side Setback from Internal Street 5-15 feet
- Side Setback from Internal Lot Line min. 5 feet
- Rear Setback from Internal Lot Line min. 10 feet

HEIGHT GUIDELINES

- Overall Height (Stories) 1.5-2.5
- Floor-to-Ceiling Height
10-14 feet — Ground Floor
10-12 feet — Second Floor



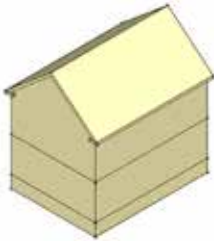
APPLIED STOREFRONTS WITH LARGE WINDOWS



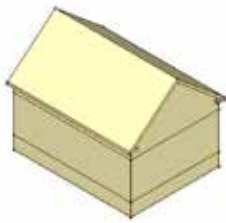
Composite Forms

BUILDING MASSING

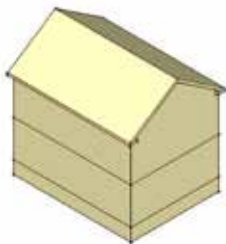
Primary Building Mass



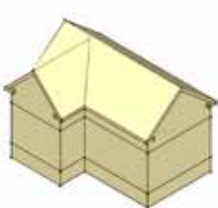
Two-story front gable



One and one-half story side gable

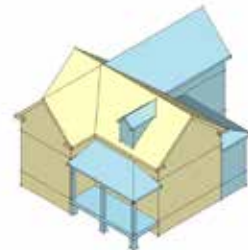
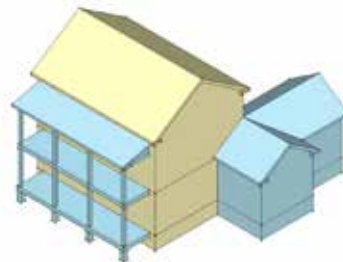
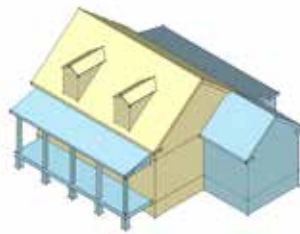
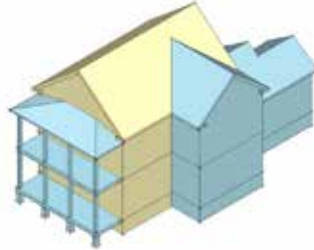


Two-story side gable



One and one-half story 'L' shape

Massing Composition With Additive Wings



Design Possibility



EAVES, DOORS, AND WINDOWS



Lowcountry eaves should be high enough to allow for the porch roof to die into the main body several inches below the eave line.



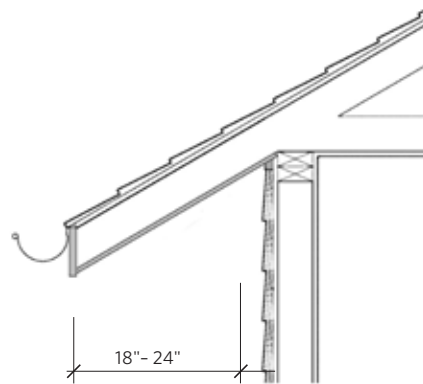
A typical boxed eave return condition at the main body of the house.



Eaves

Flat boxed eaves or open eaves with simply cut rafter tails are typical.

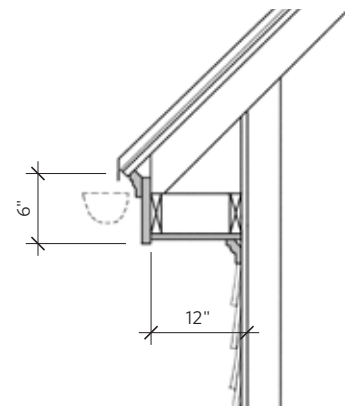
A telling feature of the Lowcountry style is that the porch roof begins just below the eave-line of the main body roof. It is typically not a continuation of the main roof.



Closed Sloping Eave

Roof

Lowcountry roof slopes vary from 6- to 10-in-12. Roofs are preferably clad in standing seam or 5V metal; however architectural shingles are permissible.



Boxed Eave with Trim

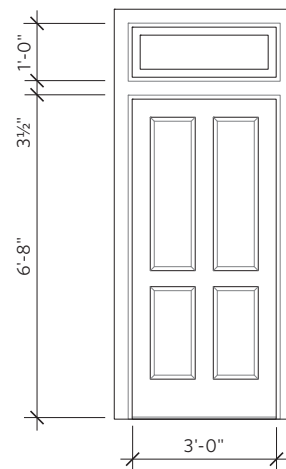
Standard Doors

Doors are centered in their bays and are either paneled or glazed. Colonial trim surround is often used. Door styles include:

4- or 6-panel style and rail doors with transom

Pair of 2/3 light stile and rail doors with transom and shutters

Standard doors are either 8 feet tall or 6 foot 8 inches with a transom. Paired doors are typically 5 feet wide and 8 feet tall.



Single Door with Transom

Standard Windows

Windows are typically square to vertical in proportion. Single windows are double-hung appearance with muntins in 6-over-6, 9-over-9, or 2-over-2 patterns if true muntins. Single pane sash over sash without muntins

Shop Windows

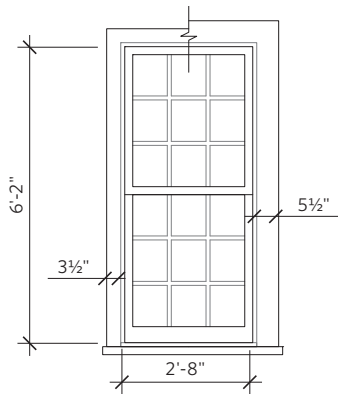
When residential building types are adapted to feature storefronts, they can be treated as either bay windows or oversize

Trim

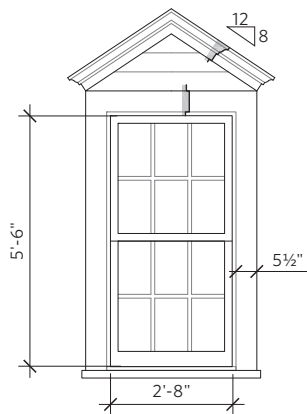
Windows and doors typically have 5/4 x 4 or 5/4 x 6 trim with a flat profile. Intermediate jambs of grouped windows have 5/4 x 4 minimum flat trim.

Shutters

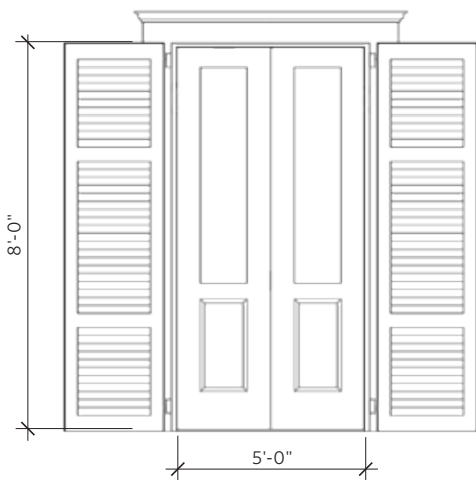
Louvered shutters are common on single windows and French doors. If used, Shutters must appear to be operable and sized to cover the sash opening.



First Floor



Dormer Window



Double Door with Shutters



Typical storefront treatment with large display windows above knee walls framed by pilasters with signboard area above

PORCHES

Porches may be broad, full-front, or wrap around additions to the house or simple porticoes. Minimum porch depth is 8 feet; porticoes may be shallower. Roof forms have shallow slopes and are tucked below the main body roof.

Columns for this style are frequently 6- to 8-inch square posts, with optional cap and base trim. Other possibilities include 8- to 10-inch square

paneled columns with simplified classical capitals and bases. Column bays are vertically proportioned.

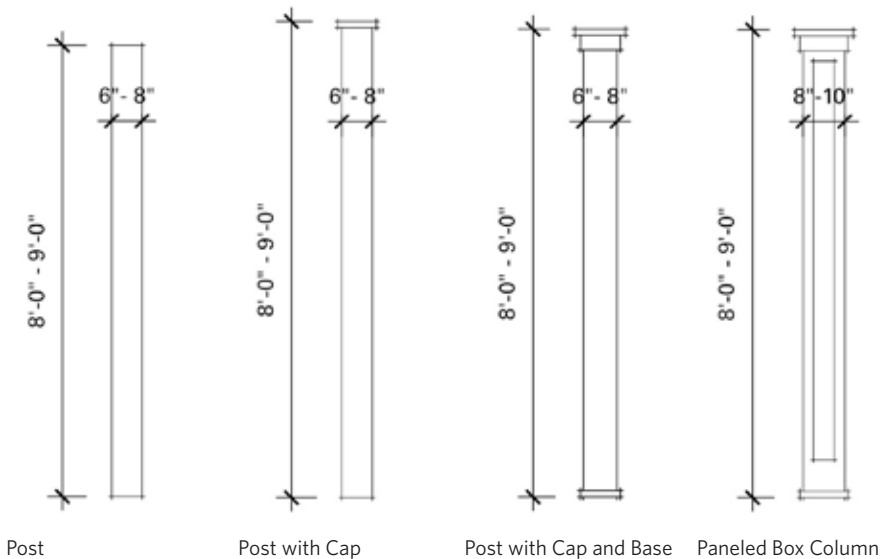
Porch railing assemblies consist of 1½ inch square picket railings with a robust milled top rail. Pickets may be spaced evenly or in repetitious patterns.

Steps are wood, composite, brick, cast stone, or stone.



Simple, classically-inspired details like those on this porch identify an Early Settlement house.

Illustrative Column Types

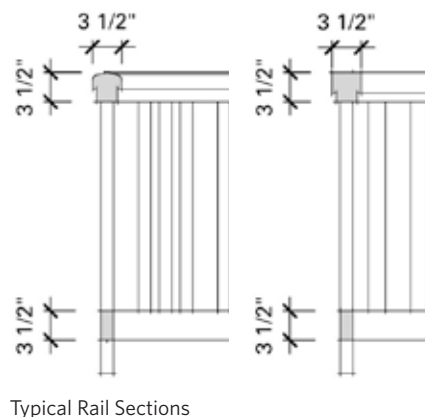


Simple single bay portico porches are typical of the Early Settlement style.

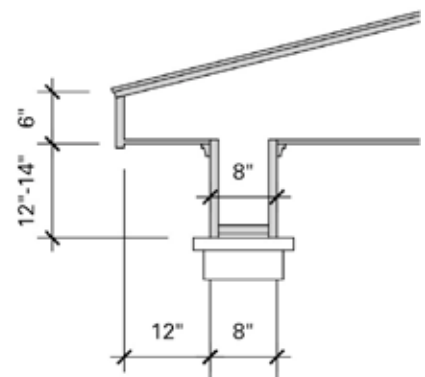
Railing Details



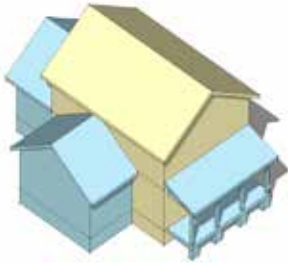
Local example of an elegant, turned baluster with substantial top and bottom rails.



Eave Section Detail



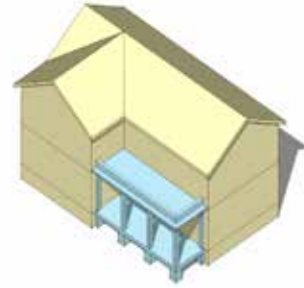
Porch Placement Diagrams



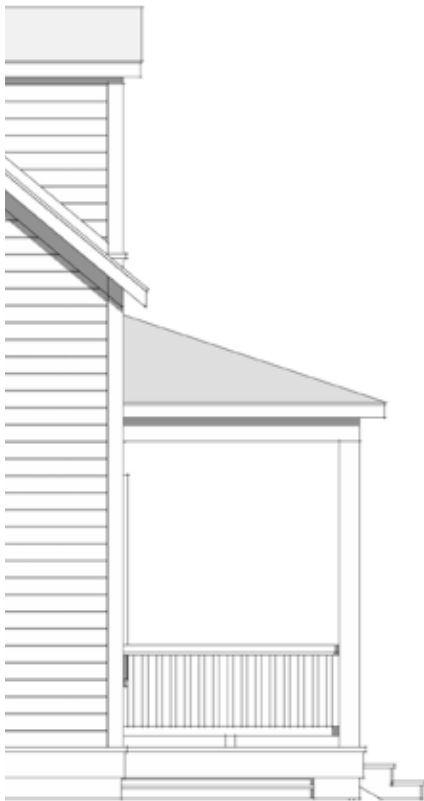
Two-story front gable with full front porch



One and one-half-story side gable with full front porch



Two-story L-shape with porch tucked into the inside corner



Porch Side Elevation



Porch Front Elevation

ANCILLARY BUILDING TYPES

Support buildings such as storage facilities, restrooms, utility buildings and garden structures can create great variety in scale and character within a development. These buildings typically follow the basic proportional characteristics of Lowcountry architecture with column types, window types, and roof pitches and eave detailing consistent with the larger building architecture and material standards.

The architectural vocabulary for these types focuses on traditional Lowcountry design elements from the Colonial and Picturesque patterns found in the region.

Garage



Shed



Public Amenity Building



MATERIALS AND COLORS

Cladding

Wood or fiber-cement smooth finish lap siding (4 to 7 inch exposure) with a beaded profile recommended, board-and-batten, or patterned shingles.

Brick to match historic Charleston brick colors and types - Old Carolina Brick Co. or equal.

Trim

5/4 thick wood, cellular PVC, or composite.

Roofing

Main body roof: Standing seam, 5V crimp metal, or metal diamond shingles recommended. Dimensional asphalt shingles accepted on case-by-case review. Flat roofs permitted with parapets or architectural enclosures to screen view of any mechanical equipment

Porches and bays with shed or hipped roof: Standing seam, 5V crimp metal or metal diamond shingles preferred.

Soffits, Fascia, and Gutters

Wood, fiber-cement, or composite soffit and fascia. Gutters are optional. Pre-finished metal half-round gutters recommended; ogee gutters permitted.

Foundations, Piers, and Chimneys

Brick or stucco with smooth or tabby finish. Gaps between piers are infilled with patterned brick or lattice.

Windows

Wood, metal, or clad with traditional window profile that sites proud of the window frame and an expressed sill. Double-hung windows or storefront windows required on street facing facades on side and rear facades.

Doors

Wood or fiberglass with traditional stile-and-rail profile.

Shutters

Wood, fiberglass, or composite with louver or board-and-batten profile. Size shutters to match sash opening and mount with appropriate hardware, preferably operable.

Railings

Wood, composite, or cellular PVC with square balusters or 5/4x boards in decorative sawn patterns. Painted metal.

Brackets, Spandrels, Gable Trim

Wood, cellular PVC, or composite.

Porch Ceilings:

Smooth stucco, wood, cellular PVC beadboard, or plywood with beadboard profile preferred.

Fences and Garden Walls

Picket fences in wood or composite material. No vinyl. Walls of brick or of brick piers with black metal fenced infill.

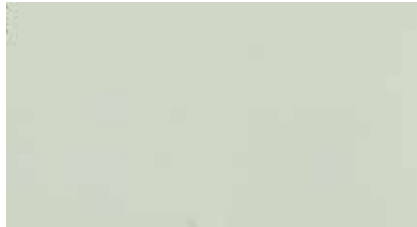
Color

Siding is white. Roof color is either Galvanized metal, Gray, Black, or Dark Red. Refer to Recommended Color Palette for appropriate colors.

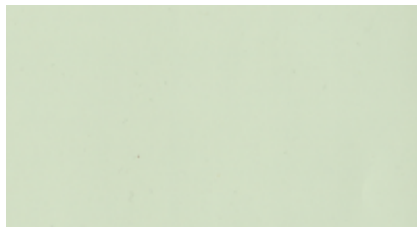


RECOMMENDED COLOR PALETTE

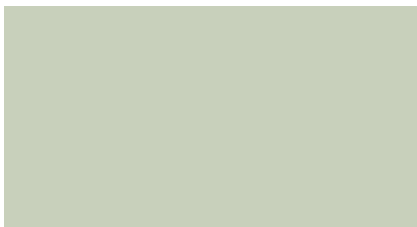
BLUE/GREEN



SW6193 Coastal Plain



BM - HC-120 Van Alen



SW6176 Liveable Green



BM - HC-116 Guilford Green

ACCENT



BM - HC-144
Newburyport



SW2801 Rookwood
Dark Red/2847
Roycroft Bottle

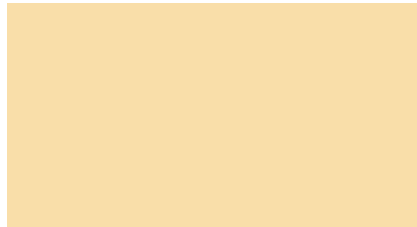


SW2846 Roycroft
Bronze Green



SW2816 Rookwood
Dark Green

YELLOW/GOLD



SW6387 Compatible Cream



SW6674 Jonquil



SW2865 Classical Yellow

ACCENT



SW2801 Rookwood
Dark Red



SW6258 Tricorn
Black

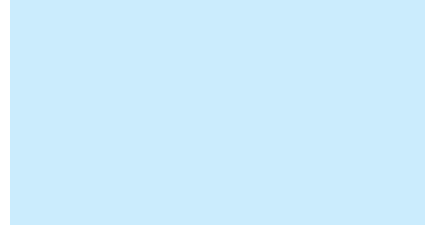


SW2846 Roycroft
Bronze Green

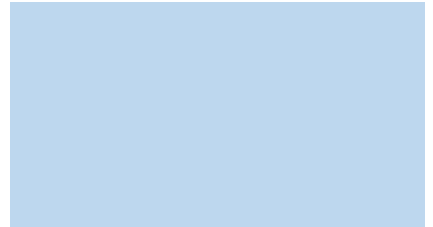


SW2847 Roycroft
Bottle Green

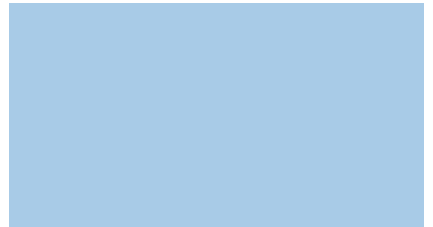
GRAY/BLUE



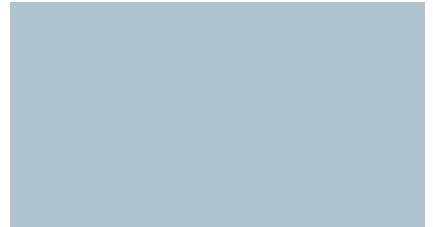
SW6799 Soar



SW6513 Take Five



SW6506 Vast Sky



SW6226 Languid Blue

ACCENT



BM - HC-144
Newburyport



SW6230 Rainstorm



SW2819 Downing
Slate

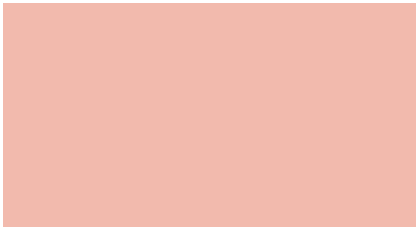


SW6258 Tricorn
Black

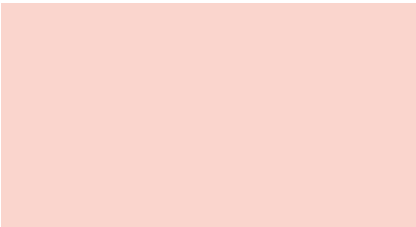
ROSE/TERRA COTTA



BM - HC-58 Chippendale Rosestone



SW6331 Smoky Salmon



SW6330 Quaint Peche



BM - HC-60 Queen Anne Pink

ACCENT



SW2801 Rookwood Dark Red

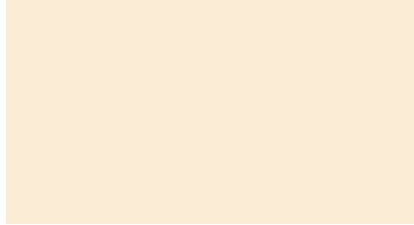


SW0006 Toile Red

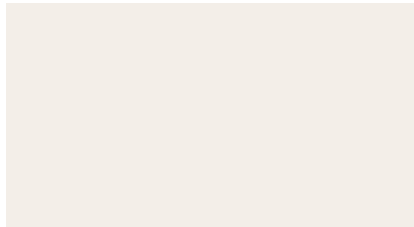


SW2803 Rookwood Terra Cotta

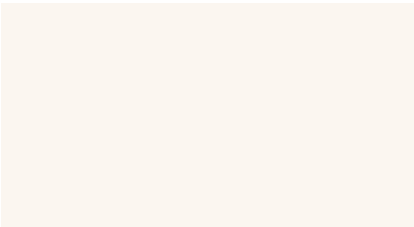
NEUTRALS/WHITE



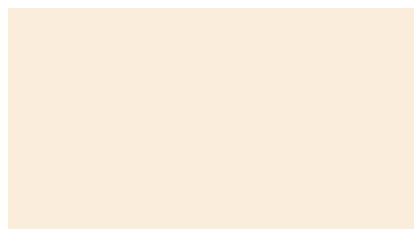
SW2833 Roycroft Vellum



SW7008 Alabaster



SW0050 Classic Light Buff



SW2829 Classical White

ACCENT



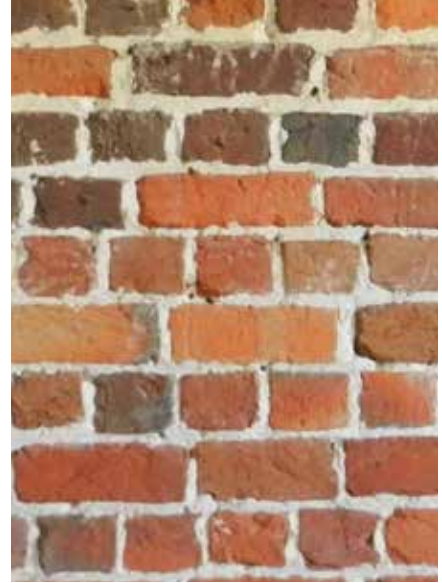
SW6174 Andiron
SW2847 Roycroft Bottle Green
SW2809 Rookwood Shutter Green



SW6258 Tricorn Black
BM - HC-144 Newburyport
SW 2819 Downing Slate



SW2801 Rookwood Dark Red
SW0006 Toile Red



Mortar for brick is recommended to be cream in color, similar to that in the historical plantation and Summerville buildings. Mortar should not be portland grey.

STORAGE, LOADING, AND UTILITY AREA SCREENING

All new developments must provide high-quality screening for external appliances and other mechanical equipment, such as trash receptacles, utility cabinets, outbuildings, and heating and air conditioning units, from public view through architecture and landscape design elements.

Guidelines for Screening Loading Bays

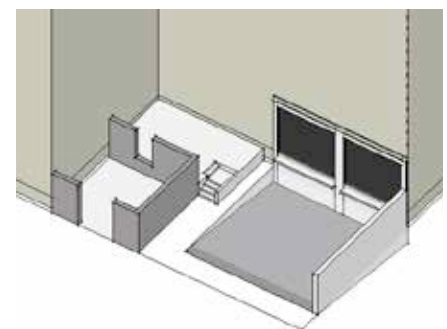
- Locate off-street loading bays to have appropriate access, with minimal interference with traffic movement.
- Create off-street loading bays that are independently accessible, so that no loading bay blocks another loading bay.
- Locate trash removal facilities and other structures so that they do not interfere with loading areas.
- Locate loading bays at the rear of buildings, with architectural walls and evergreen landscape elements.
- Screen loading docks, dumpsters, and other back-of-house requirements from direct view from streets.



Examples of loading docks on large format commercial buildings, screened from view



Example of trash receptacle screening



Screened trash and loading bay in the rear of building

Guidelines for Screening Mechanical Equipment

- Use the form of the roof, cornice, or screen to hide mechanical equipment, plumbing stacks, and vents, from public view.
- Consider topographic conditions and views from the public rights-of-way and primary access lanes for each spatial type.
- When at ground level, locate mechanical systems at the rear of the building, away from the public realm.
- Screen mechanical equipment by hedgerows, low walls, or fences and create wrap-around landscape buffering.

References

For detailed requirements on screening, reference:

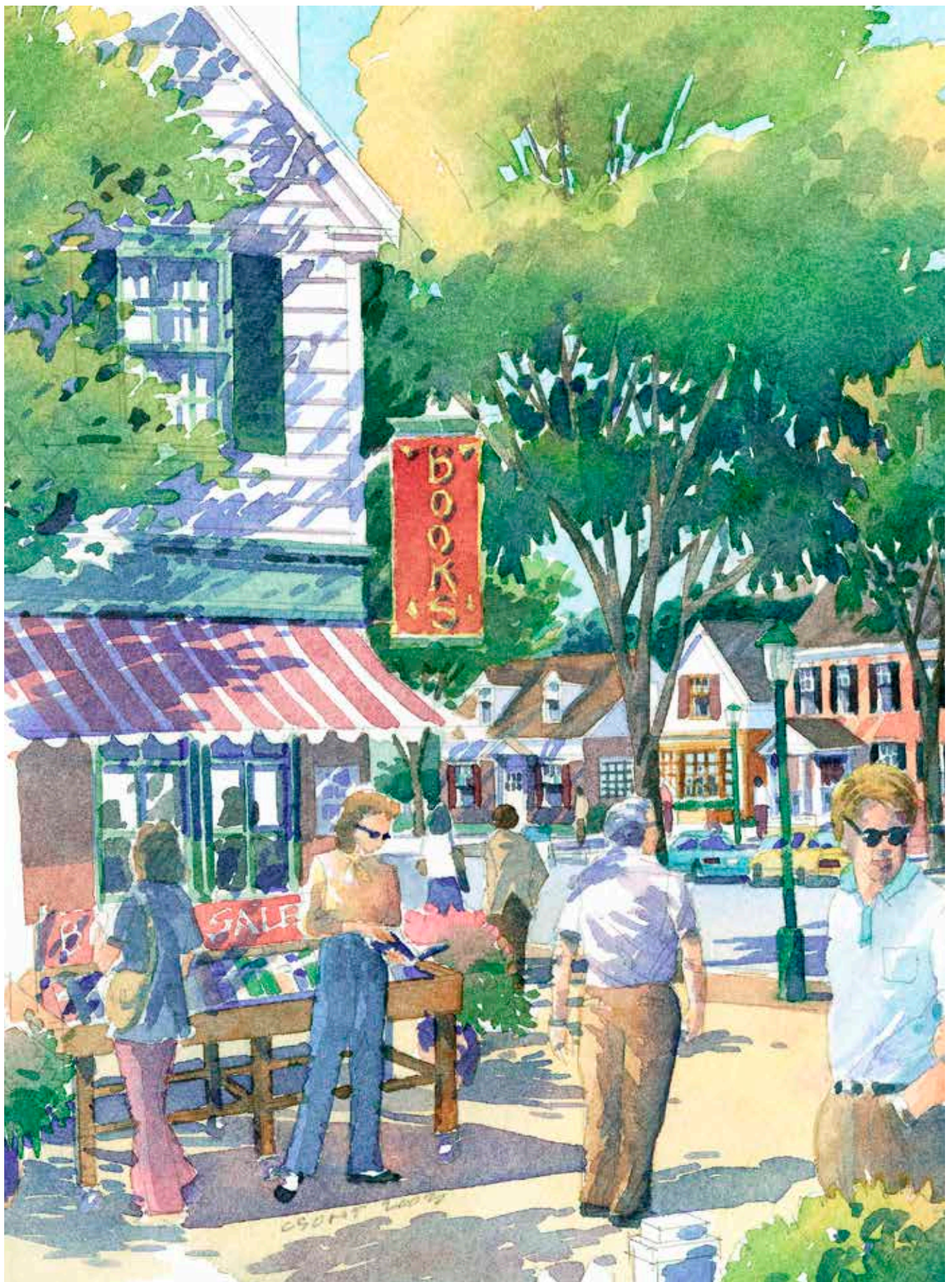
- [Article XI. Special Areas](#)
- [11.2.6 Screening and buffering certain site features and accessory uses](#)
- [11.2.8 Development Design and Architectural Review](#)
- [11.2.8 \(7\) Storage, Loading and Utility Area Screening](#)

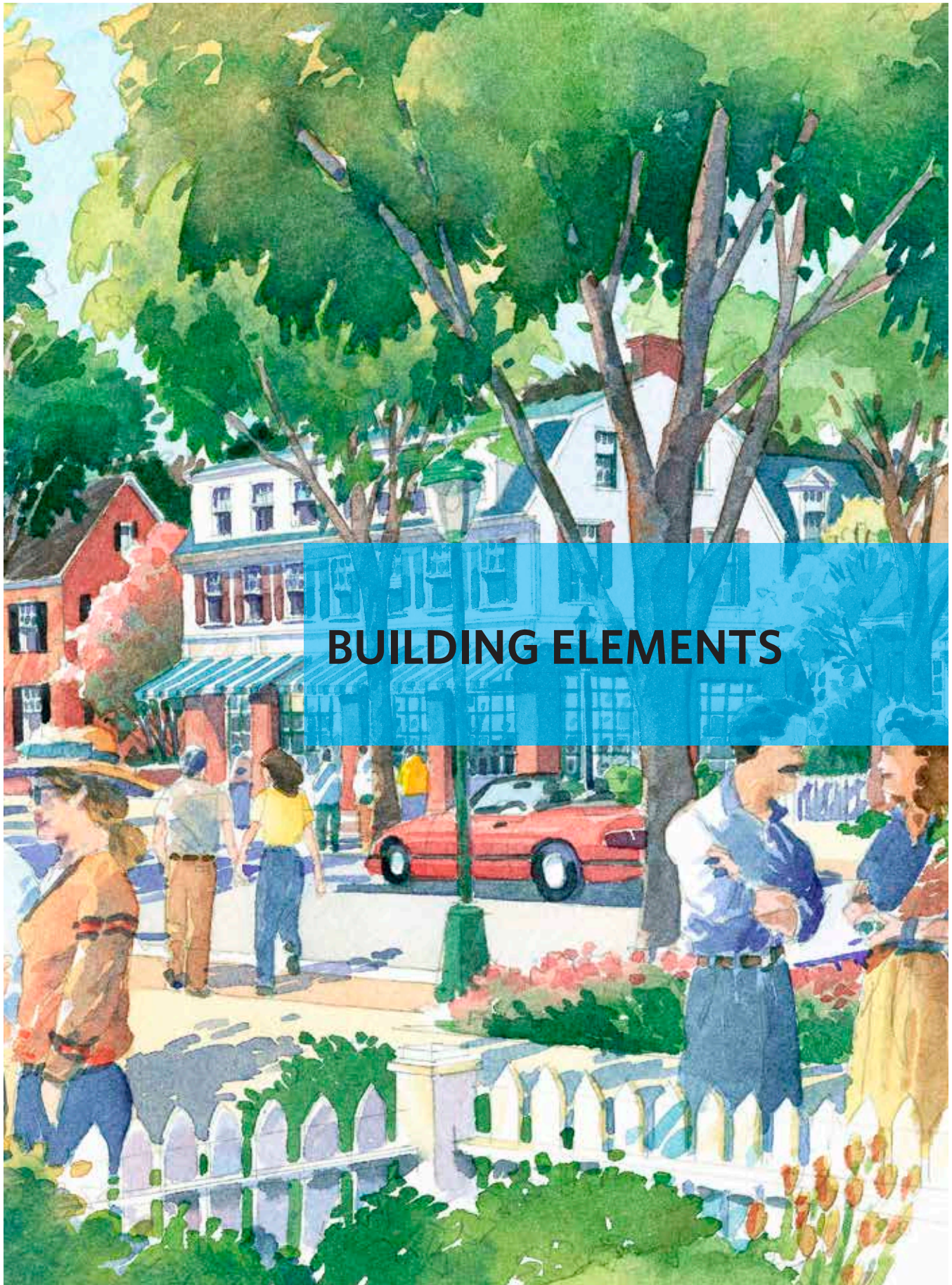


Utilities screened on the side of the building



Architectural screening elements built from high-quality materials to match buildings





BUILDING ELEMENTS

STOREFRONTS

For commercial buildings in Cooks Crossroads, particular attention must be paid to the design of storefronts and to the relationship between exterior and interior space. The design of storefronts is critical to the success of the pedestrian realm and will impact the sense of security and safety in a neighborhood. The following are a series of elements that must be considered:

Storefront Composition

- Ground-floor retail spaces have a minimum height of 14 feet from floor to floor.
- Storefront design must utilize the full height of the ground-floor facade frontage.

Awnings

- Awnings are encouraged and may provide additional signage space by incorporating names and logos.
- Plastic, over-scaled, and fluorescent back-lit signs and awnings are not permitted.

Openings and Transparency

- Glazing or window trim should constitute a minimum of 60% of the ground-floor retail facades.
- Shop windows should provide views into the shop as well as its displays.

Lighting

- Goose-neck or other mounted lights are encouraged.
- LED lights are preferred
- The fixtures shall be primarily down-firing. Up-firing fixtures, if used for aesthetic effect, must be designed to prevent Dark Sky intrusion.

UNDESIRABLE STOREFRONTS

- Cluttered shop windows with reduced or impeded visibility between the interior and exterior
- Fluorescent lighting
- Back-lighted signs and awnings
- Solid security gates
- Reflective glazing
- Inaccessible entries
- Garish paint colors



STOREFRONT COMPOSITION

- Storefronts shall be designed using traditionally framed elements of retail design as well as innovative new components that emulate the composition of traditional retail design. Characteristic elements include large transparent display windows with kick plates below and clerestory windows above, recessed front entries, and exterior awnings and signs.
- Multiple storefronts within the same building shall be visually compatible in terms of scale, alignment, and their relationship to the building as a whole, yet distinguished between various shops using storefront design, color, signage, and awnings. The coherence of the building design should be able to accommodate the diversity of character and individuality amongst various shops in one building.
- Storefronts shall maintain a typical rhythm wherever possible, such as 10 to 20 or 15 to 30 feet wide at the ground level, each with its own entry.
- Storefront entrances shall be clearly distinguished from those serving floors above.
- Kick plates, windows, transoms, clerestories, signage bands, upper floor windows, and cornices shall align where possible but should allow for a level of vertical, horizontal, and three-dimensional variations at the lower level to help create a variegated and organic quality for the storefronts.
- Windows must have at least 60% transparency into the interior.
- Individual storefronts shall be distinguished by interesting design features at the ground level such as lighting, medallions, belt courses, plinths for columns, piers or pilasters, projecting sills, tile work, stone or concrete masonry, pedestrian-scaled signs, planter boxes, and specialty bay windows.
- Within the compositional framework above, storefronts may be composed of various kinds of operational doors and windows that will encourage the opening up of interior spaces onto the sidewalks and terraces, including French doors, modified garage doors, sliding doors, walk-through double- and triple-hung windows, and others that will support the opening up of interior spaces to the outside.
- Shadow boxes less than 36 inches deep are prohibited.



MATERIALS

- Brick, stone, cast stone, ceramic tile, hard coat stucco, wood, wood substitute (smooth finish, cementitious planks, and panels or cellular PVC) or pre-finished heavy gauge metal panels are preferred. Entrance doors shall generally be clear glass in wood or metal frames. Standard, industrial aluminum storefronts are not appropriate. Provide variation in materials and colors for larger buildings, to break down the scale of buildings and to reflect the general size and scale of heritage buildings.
- Storefront windows typically consist of large, transparent plate glass set in wood, clad wood, or metal frames. Display windows must incorporate high transparency; windows must have high visibility transmittance values (37% or more) and low daylight reflectance (15% or less). Colored, visibly tinted, or mirrored glazing is not appropriate.



STOREFRONT SIGNAGE

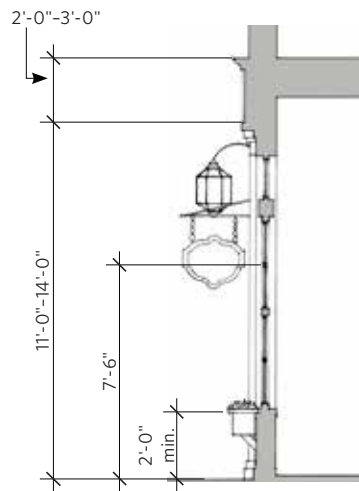
The location of storefront signage is important for a commercial street. It invites the pedestrian into the store and promotes an active retail area.

- Signage should be visible by the pedestrian from the sidewalk.
- Signage could be embossed or beveled within the frieze board above the store window or entrance.
- Signage could hang perpendicular to the storefront by bracket and chains in order to face pedestrian traffic.

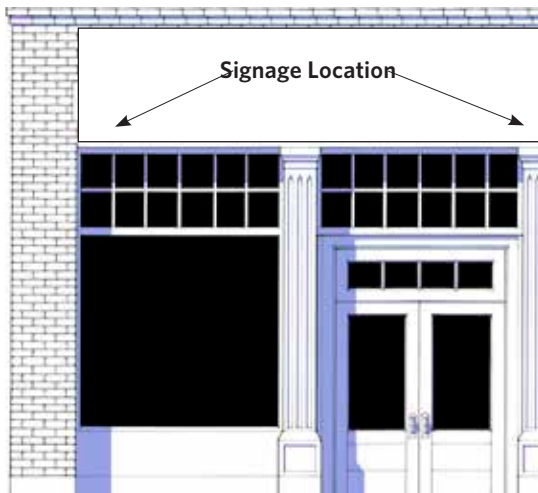
Storefront Design



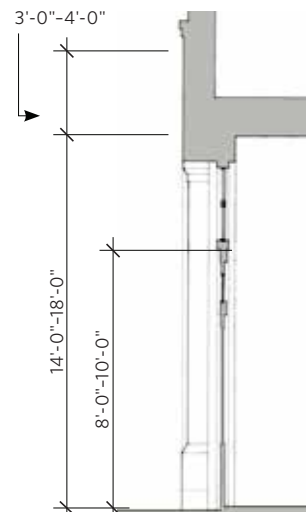
Adapted Residential Building Storefront

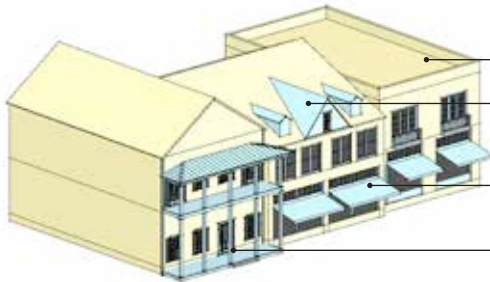


Colonial Revival storefront



Mercantile Storefront





VARIED ROOF HEIGHTS AND PITCHES

RESIDENTIAL BUILDING ELEMENTS

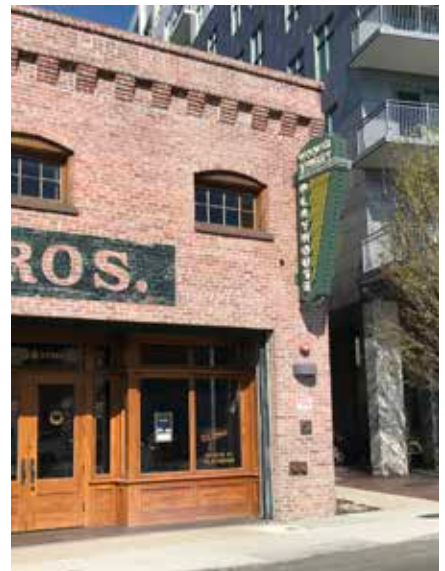
STOREFRONT AWNINGS

ONE OR TWO STORY PORCHES



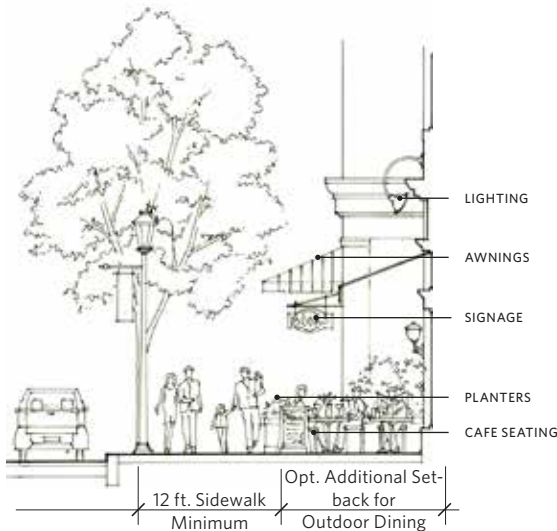
FLAT ROOF WITH PARAPETS

APPLIED STOREFRONTS WITH LARGE WINDOWS



SIDEWALK AREAS

- Retail, restaurant, and other food and beverage operators are encouraged to design and operate exterior sidewalk areas in a manner that will create a seamless connection from their interior operation to the exterior spaces.
- At sidewalk setback locations, storefront designs that reinforce the connection between inside and outside are encouraged. Storefront designs can use operational doors and windows that allow for direct connection and movement between the sidewalk and restaurant and shop interiors.
- Restaurants and shops are encouraged to maximize the use of exterior space as an extension of interior activity.
- The use of outdoor plantings, planting boxes, and flower boxes is recommended in the supplemental setback areas.
- Awnings, canvas umbrellas, and heat lamps can be used to extend the seasonal use of sidewalk areas.



COMMERCIAL BUILDING SIGNAGE

Signs act as important elements of buildings to identify the presence and nature of various types of residential and commercial establishments. Proper sign design will also enliven and enrich the streetscape experience for pedestrians, without detracting from the spatial coherence and quality of public spaces and the streetscape. The following guidelines are intended to identify the sign types appropriate for Cooks Crossroads. All signs:

- Must be in accordance with County regulations. These guidelines are intended to add further refinement to the County sign ordinance. They are not intended to lessen the jurisdiction of the ordinance.
- Should express quality and unique characteristics.
- Must be legible and easily understood.
- Should contribute positively to the sense of place and the character of the district.
- Should communicate the nature of the corresponding business.
- Must reinforce pedestrian scale in size and mounting height.
- Should reflect proper purpose, context, and location.



PROHIBITED SIGNS

The following signs are prohibited:

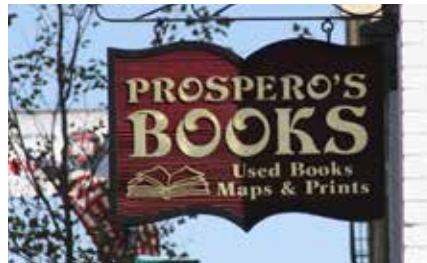
- Any “box” or “can” letters or signs (internally-lit boxes with translucent covers).
- Any sign illuminated by bare floodlight, blinking or flashing bulbs.
- Any sign that extends above the roof line or parapet wall.
- Any sign erected, painted, or maintained upon fences, rocks, trees, or any natural feature.
- Electronic message signs with changing text or graphics generated by electronic components.

SIGNAGE REQUIREMENTS

The following items should be noted prior to any sign design or building development:

- Multiple storefronts within one building must be designed to coordinate the alignment of signs to achieve a harmonious appearance to the base of the building.
- Signs must fit within the architectural divisions of the building, and should not span across structural bays or columns.
- If present, the signage band must be incorporated into the design of the facade and situated above the storefront clerestory and below the second-story windows.

- Signs incorporated in cornices or parapets must be limited in size, or made an integral part of the architecture.
- Signs only lit by exterior wall-mounted fixtures or signs with individually shadow-lit opaque letters are permitted.
- The inclusion of creative shapes, symbols, and three-dimensional motifs with accompanying text is strongly encouraged in all sign design.



IDENTITY SIGNS

The following types of Identity Signs are permitted:

Flush-Mount Wall Signs

Flush-mounted wall signs are a fundamental form of identity signage. They are attached flat to the wall and form an integral part of the architectural detailing of the building. The signs should be positioned within logical features of the building, such as transom panels above entryways, signage bands above storefronts, or wall panels next to storefront windows. Signs should typically be located between 4 to 6 feet above the adjacent walking surface or in the architectural sign band located above doors and shops.

Blade Signs (including Projecting Bracket-Mounted Signs)

Blade signs are typically suspended from a permanent bracket attached to the building, either fixed in place or suspended from the bracket. They are great opportunities for creating colorful, unique identities for businesses. Blade signs should be no larger than 12 square feet per side and set at a minimum clearance of 8 feet above sidewalk level. Blade signs should not extend more than 4 feet from the building.

Window Signs

Window signs are directly applied vinyl or paint to the window or door glass of the retail storefront. Window signs are another great artistic opportunity and should be of high quality. They should be no larger than 25% of the window area on which they are placed.

Suspended Signs

These signs are suspended from overhead structures of pedestrian walkways or underneath an arcade. Signs perpendicular to the building facade should be a maximum width of 3 feet and shall not extend beyond 4 feet from the building facade. Signs parallel to the facade shall not be greater than 12 feet in width. All suspended signs should be a minimum of 10 feet above the sidewalk.

Awning Signs

Awning signs are characterized by varying colors and sizes to differentiate businesses. They also provide shade and cover from the rain. Awnings tend to be fabric as opposed to canopy signs which may be structural. Awnings shall not be internally illuminated, but may be illuminated by a direct exterior lighting source located above the awning and attached to the building exterior. Translucent materials and plastics are not permitted as awning materials. Lettering, emblems, or logos are permitted on the awning valance.

Murals

Wall murals should be designed to create welcoming environment turning a blank wall into fun element in a space. The content should be in consideration of surrounding environment and design schemes with elegant and family-friendly designs.



FREESTANDING SIGNS

Canopy Signs (Including Marquee Signs)

These signs or sign letters are attached to, painted on, or inscribed upon a projecting canopy or marquee. The canopy is typically comprised of structural elements. The canopy or marquee is generally characterized by a three-dimensional quality and by its ability to accommodate lettering on two sides, if vertically mounted like a traditional movie marquee, or on three sides if horizontally mounted. The minimum clearance above a pedestrian sidewalk is 8 feet.

In addition to permanent “Identity Signs,” individual businesses are also permitted to have “Freestanding” signs. Freestanding signs should not be considered to be

Identity Signs and should not count as part of the permitted allotment for a ground-floor business:

Menu Board Signs

These are building-mounted signs with a cleanly presented menu for the adjacent eating establishment.

Sandwich or A-Frame Signs

These are double-faced, freestanding signs that may have permanent or erasable information on both sides. Sandwich or A-Frame Signs are designed as a place to identify daily specials or special items not typically included on a menu or Menu Board Sign. They are also used to help identify businesses located down alleyways or in courtyards off of the primary frontage. Sandwich and A-Frame Signs shall not be placed within the pedestrian area of the sidewalk.

Table Umbrella Signs

Similar to awnings, table umbrellas or parasols should be used by eating establishments to add vibrant color to outdoor spaces and to create attractive outdoor seating areas. Restaurants may use the table umbrellas to enhance the identification of their business through the selection of thematic colors or by adding text or logos directly onto the umbrellas. A restaurant may have a number of table umbrellas which should be consistent in design.







LANDSCAPE PATTERNS

GATEWAY LANDSCAPE ELEMENTS

The historic and scenic value of the Ashley River Corridor is largely contributed to by the tree canopy and sensitive natural landscapes adjacent to the River. In thinking about development at Cooks Crossroads, complete care and attention must be paid to the landscape approach.

Preservation

The landscape buffers along SC Highway 165 and Ashley River Road serve the objective of preserving and enhancing the tree canopy and natural landscape along these corridors. In addition to the buffers, a preservation approach should be taken to developed areas of each parcel, using site walls and compact development to minimize grading and clearing.

Rural and Informal

Within developed areas, the language of the landscape design should be rural and informal, rather than manicured and suburban. The landscape should be natural and simple, referencing village development in Dorchester County. Much like Summerville, a garden aesthetic is appropriate around small commercial buildings and shared spaces.

References

- [13.3.4 Required Canopy Coverage](#)
- [13.3.5 Required Landscape Elements](#)



SC Highway 165 and Ashley River Road Buffers
Landscape buffers along the two arterials create the scenic character of the Crossroads.





Ashley River Corridor Landscape Character

Tree canopies greatly influence the scenic character of the Ashley River Corridor. Their preservation is essential in Cooks Crossroads.



LANDSCAPE PATTERNS

LANDSCAPE BUFFER REQUIREMENTS

Multiple landscape buffers apply to the properties of Cooks Crossroads. The buffers protect the historic and scenic view corridors and environmental elements in the District. The buffers that apply within District 5 include:

1. Transitional Overlay District (TOD) Minimum Visual Buffer (MVB)
2. Ashley River Corridor Overlay District Buffer
3. Ashley River Historic Overlay District Buffer

TOD Minimum Visual Buffer (MVB)

The MVB is intended to reduce the impact of structures and parking areas, screening headlight glare, and lessening light spillover from sight lighting. Development, clearing, grading, and construction are prohibited within the buffer, with the exception of approved access ways, infrastructure installation, and plantings required by the Planning Department.

The MVB Buffer is equal to 8% of the average depth of the parcel of the proposed development, with a minimum width of 15 feet and a maximum width of 40 feet along SC Highway 165 and Ashley River Road.

References

To determine which properties are within the TOD and for specific buffer standards, please refer to:

- [11.2.2 Establishment of the Transitional Overlay District](#)
- [11.2.6 Minimum Visual Buffer \(MVB\) Standard](#)

Ashley River Corridor Overlay District Buffer

The Ashley River Corridor Overlay promotes responsible development and mitigates the impacts of new construction within the overlay to safeguard local heritage, natural beauty, and economic and recreational resources, conserve wildlife habitat, stabilize riverbanks, and protect water quality and marine nursery resources.

A river front buffer of 50 feet applies to all other riverfront properties in the Ashley River Corridor Overlay.

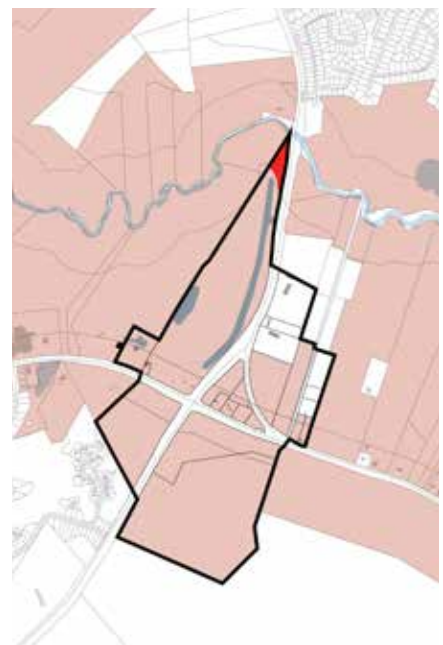
References

To determine which properties are within the Ashley River Corridor Overlay and for specific buffer standards, please refer to:

- [11.4.2 Area of Overlay](#)
- [11.4.4 Supplemental Design and Protection Standards](#)



Transitional Overlay District (TOD)
The properties within the TOD and the MVB are shown above.



Ashley River Corridor Overlay District
The properties within the Ashley River Corridor Overlay District and the corresponding buffer are shown above.



Ashley River Historic Overlay
The properties within the Historic Overlay and the corresponding buffer are shown above.

Ashley River Historic Overlay District Buffer

The Ashley River Historic Overlay District was created to protect the natural and historic character of the Ashley River Historic Overlay, with similar objectives to the Ashley River Corridor District. Lots 1500 feet in depth or greater have a required buffer of 200 feet. Lots less than 1500 feet in depth have a buffer equal to 13.3% of the depth of the lot, in no case less than 50 feet in depth.

References

To determine which properties are within the Ashley River Historic Overlay and for specific buffer standards, please refer to:

- [11.5.7 District 5](#)
- [11.5.8 Requirements Imposed Upon All Districts](#)

Within the Crossroad, mosts properties are within the Ashley River Historic Overlay District, and therefore have a buffer ranging from 50 to 200 feet.

General Buffer References

- [Section 12.4 Tree and Canopy Protection Standards in Dorchester County Zoning and Land Development Ordinance.](#)
- [13.2.2 Use Buffer Types](#)
- [13.2.3 Height-to-Depth Setbacks \(Type "H" Buffers\)](#)
- [13.2.4 Right-of-way Buffering](#)



Combined Buffers and Setback Constraints
The diagram above shows all of the buffers that impact the District 5 properties, in addition to the 50-foot setbacks from all property lines.



Wetlands and areas impacted by buffers within Rosebrock Park

ON-SITE STORMWATER MANAGEMENT OPTIONS

Flood Areas

Properties north of Ashley River Road are impacted by the Floodway and Floodplain of the River. Finished floor levels must be raised above the Floodplain elevation and all precautions should be taken to develop sensitively within these areas, accounting for challenges in accommodating stormwater management and flood mitigation.

Reference: [12.2 Flood Hazard Areas](#)

Wetlands

While wetland mapping exists at the County level, verification of the existing wetland conditions must be confirmed through a survey. Development and stormwater must be located outside of the wetland area.

Reference: [12.3 Wetlands Protection](#)

On-Site Design Accommodations

Developers are required to handle stormwater on-site. Within Cooks Crossroads, there is an expectation that partners will work collaboratively with County Public Works to establish the toolkit of on-site stormwater management strategies that are appropriate for a given site's specific conditions.

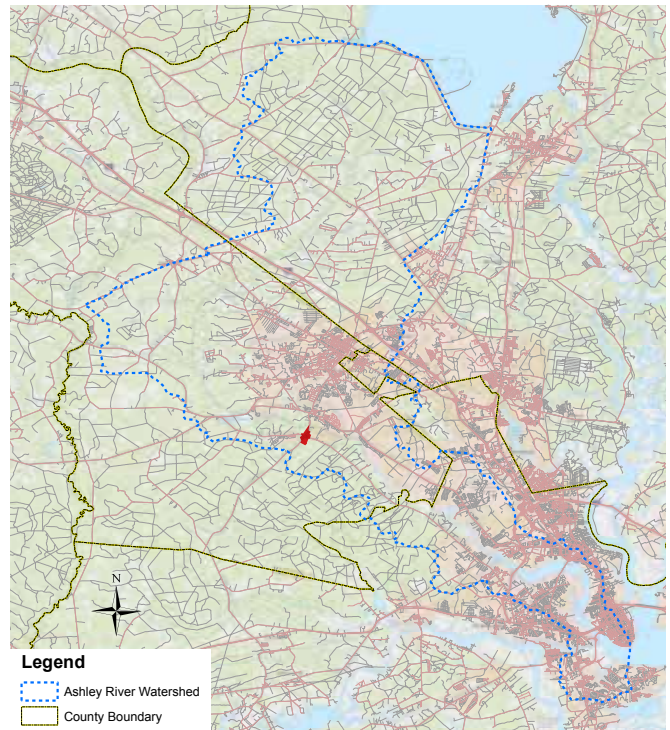


Rain gardens may help to handle stormwater on-site, while providing visual amenities to development.



Ashley River Floodway and Floodplain

The floodway and floodplain impact District 5 properties north of SC 61.



Ashley River Watershed

The floodway and floodplain impact District 5 properties north of SC 61.

On-Site Design Accommodations

Developers are required to handle stormwater on-site. Within Cooks Crossroads, there is an expectation that partners will work collaboratively with County Public Works to establish the toolkit of on-site stormwater management strategies that are appropriate for a given site's specific conditions. Development of Cooks Crossroads will have a crucial impact on the adjacent Ashley River and the canal through Rosebrock Park that feeds directly into the River, so stormwater management is of highest importance for the protection of the scenic river.

Stormwater and flood prevention should be one of the most important development considerations, as it impacts the health of the scenic river, wildlife habitat, and flooding for surrounding properties.

Innovation in green infrastructure and creative approaches to stormwater management should be employed, in addition to the standard set of tools. All interventions should be designed to be in keeping with the gateway character, including:

- Rain gardens
- Bioswales
- Wildlife habitat creation

Mitigation

Off-site mitigation should be minimized and filling in of wetlands in Cooks Crossroads is strongly discouraged. Where mitigation is agreed upon as an appropriate strategy by the County and the developer:

- Mitigation should be localized within the same sub-watershed so that the impacts are mitigated for the Ashley River to balance the effects of development.
- Parcels adjacent to the Ashley River should be purchased and used as a wetland bank for Cooks Crossroads development.

Together, these stormwater management approaches and tools are designed to advance climate and flood resiliency. They also present an educational opportunity for visitors in the gateway, helping to communicate the care and intentionality with which the area was developed. Impacts on wildlife habitat and diversity should be considered as a part of each decision, in addition to flood control and stormwater management.



Wetland Mapping

A survey should be used to accurately confirm existing wetlands on a parcel.

LANDSCAPE CHARACTER

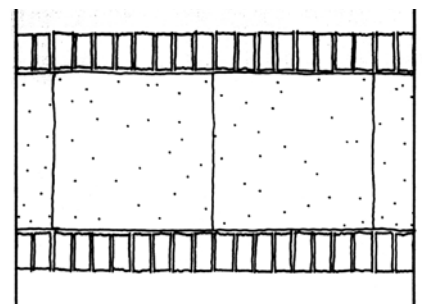
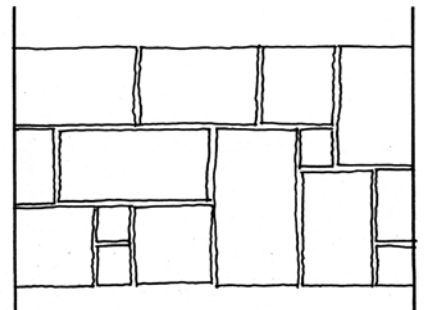
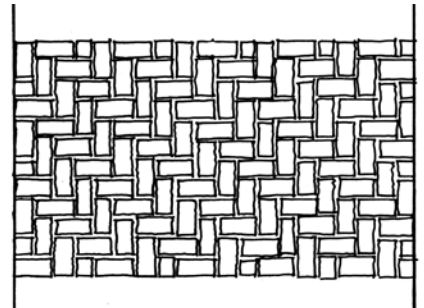
PAVING MATERIALS

Sidewalks, outdoor terraces, and courtyards are integral components of the character of the Crossroads. Wherever possible, pervious paving in hardscape areas is preferred. Acceptable paving materials for hardscape include:

- Brick pavers
- Concrete pavers/pervious pavers
- Crushed stone, gravel, or oyster shells

Acceptable paving materials for streets and shared lanes will be dependent on ownership, use, and maintenance responsibilities for streets. Possible materials may include:

- Exposed aggregate concrete
- Bituminous asphalt with rolled shot-and-chip



NATIVE PLANT PALETTE

New landscape at Cooks Crossroads is expected to be native, bird habitat-friendly, and contributing to the overall gateway character.

RECOMMENDED PLANT PALETTE

Common Name	Botanical Name	Common Name	Botanical Name
Recommended Trees			
Hedge Maple (D)	<i>Acer campestre</i>	Slash Pine (E)	<i>Pinus elliottii</i>
Red Maple (D)	<i>Acer rubrum</i>	Longleaf Pine (E)	<i>Pinus palustris</i>
River Birch (D)	<i>Betula nigra</i>	Loblolly Pine (E)	<i>Pinus taeda</i>
Mulberry (D)	<i>Broussonetia papyrifera</i>	American Sycamore (D)	<i>Platanus occidentalis</i>
American Hornbeam (D)	<i>Carpinus caroliniana</i>	Cottonwood (D)	<i>Populus deltoides</i>
Bitternut Hickory (D)	<i>Carya cordiformis</i>	Black Cherry (D)	<i>Prunus serotina</i>
Pignut Hickory (D) <i>Quercus alba</i>	<i>Carya glabra</i>	White Oak (D)	<i>Quercus alba</i>
Pecan (D)	<i>Carya illinoensis</i>	Scarlet Oak (D)	<i>Quercus coccinea</i>
Mockernut Hickory (D)	<i>Carya tomentosa</i>	Southern Red Oak (D)	<i>Quercus falcata</i>
Deodar Cedar (E)	<i>Cedrus deodara</i>	Laurel Oak (SE)	<i>Quercus laurifolia</i>
Hackberry (D)	<i>Celtis laevigata</i>	Swamp Chestnut Oak (D)	<i>Quercus michauxii</i>
Common Persimmon (D)	<i>Diospyros virginiana</i>	Water Oak (SE)	<i>Quercus nigra</i>
American Beech (D)	<i>Fagus grandifolia</i>	Pin Oak (D)	<i>Quercus palustris</i>
Green Ash (D)	<i>Fraxinus pennsylvanica</i>	Willow Oak (D)	<i>Quercus phellos</i>
Sweetgum (D)	<i>Liquidambar styraciflua</i>	Shumard Oak (D)	<i>Quercus shumardii</i>
Yellow Poplar (D)	<i>Liriodendron tulipifera</i>	Live Oak (D)	<i>Quercus virginiana</i>
Black Walnut (D)	<i>Juglans nigra</i>	Black Willow (D)	<i>Salix nigra</i>
Eastern Red Cedar (E)	<i>Juniperus virginiana</i>	Bald Cypress (D)	<i>Taxodium distichum</i>
Cucumber Magnolia (D)	<i>Magnolia acuminata</i>	Basswood (D)	<i>Tilia americana</i>
Southern Magnolia (E)	<i>Magnolia grandiflora</i>	Winged Elm (D)	<i>Ulmus alata</i>
Bigleaf Magnolia (D)	<i>Magnolia macrophylla</i>	American Elm (D)	<i>Ulmus americana</i>
Black Tupelo (D)	<i>Nyssa sylvatica</i>	Cedar Elm (D)	<i>Ulmus crassifolia</i>
Virginia Pine (E)	<i>Pinus virginiana</i>		



Pin Oak



Southern Magnolia

RECOMMENDED PLANT PALETTE

Common Name	Botanical Name	Common Name	Botanical Name
Understory Trees			
Red Buckeye (D)	Aesculus pavia	Cassine Holly (E)	Ilex cassine
Serviceberry (D)	Amelanchier arborea	Myrtleleaf Holly (E)	Ilex cassine 'Myrtifolia'
Devil's Walking Stick (D)	Aralia spinosa L.	Possum Haw (D)	Ilex decidua
Red Chokeberry (D)	Aronia arbutifolia	Inkberry (E)	Ilex glabra
Paw Paw (D)	Asimina triloba	Lusterleaf Holly (E)	Ilex latifolia
Hornbeam (D)	Carpinus caroliniana	American Holly (E)	Ilex opaca
Chinquapin (D)	Castanea dentata	Black Alder (E)	Winterberry Ilex verticillata
Southern Catalpa (D)	Catalpa bignonioides	Yaupon Holly (E)	Ilex vomitoria
Buttonbush (D)	Cephalanthus occidentalis	Sweetbay Magnolia (E)	Magnolia virginiana
Eastern Redbud (D)	Cercis canadensis	Southern Crabapple (D)	Malus angustifolia
White Fringe Tree (D)	Chionanthus virginicus	Hop Hornbeam (D)	Ostrya virginiana
Flowering Dogwood (D)	Cornus florida	Sourwood (D)	Oxydendron arboreum
Riverflat Hawthorn (D)	Crataegus opaca	Red Bay (E)	Persea borbonia
Green Hawthorn (D)	Crataegus viridis	American Plum (D)	Prunus americana
Leatherwood (E)	Cyrilla racemiflora	Cherry Laurel (E)	Prunus caroliniana
Honeylocust (D)	Gleditsia triacanthos	Black Locust (D)	Robinia pseudoacacia
Gordonia (E)	Gordonia lasianthos	Palmetto (E)	Sabal Palmetto
Carolina Silverbell (D)	Halesia diptera	Horse Sugar (E)	Symplocos tinctoria
Foster Holly (E)	Ilex attenuata 'Fosterii'		



Buttonbush



Sourwood

RECOMMENDED PLANT PALETTE

Common Name	Botanical Name	Common Name	Botanical Name
Understory Shrubs			
American Beautiberry (D)	Callicarpa americana	Native Azalea (D)	Rhododendron Species
Sweetshrub (D)	Calycanthus floridus	Sumac (D)	Rhus Species
Cliftonia (E)	Cliftonia monophylla	Elderberry (D)	Sambucus canadensis
Deutzia (D)	Deutzia scabra	Coralberry (D)	Symphoricarpos orbiculatus
Sweet Pepperbush (D)	Clethra alnifolia	Sparkleberry (D)	Vaccinium arboreum Marsh
Hearts-a-burstin' (D)	Euonymus americanus	Black Highbush (D)	Bilberry Vaccinium atrococcum
Witch Alder (D)	Fothergillia major	Deerberry (D)	Vaccinium caesium
Hazel (D)	Hamamelis virginiana	Elliot's Blueberry (D)	Vaccinium ellioti
Oakleaf Hydrangea (D)	Hydrangea quercifolia	Arrowwood (D)	Viburnum dentatum
Anise (E)	Illicium floridanum	Swamp Viburnum (D)	Viburnum nudum
Virginia Willow (D)	Itea virginiana	Weigela (D)	Weigela florida
Fetterbush (E)	Lyonia lucida	Aloe Yucca (E)	Yucca aloifolia
Wax Myrtle (E)	Myrica cerifera	Adam's Needle Yucca (E)	Yucca filamentosa
Myrtle (E)	Myrtus communis	Spanish Dagger Yucca (E)	Yucca gloriosa
Feverbark (D)	Pinckneya pubens		
Understory Grasses			
Bushy Broomsedge	Andropogon glomeratus	Switch Grass	Panicum virgatum
Broomsedge	Andropogon virginicus	Foxtail Grass	Setaria geniculata
Cane	Arundinaria gigantean	Green Bristlegrass	Setaria viridis
Reed Grass	Calamagrostis cinnoides	Indian Grass	Sorghastrum sp.
Whitetop Sedge	Dichromena latifolia	Salt Hay	Spartina patens
Sweetgrass	Muhlenbergia filipes	Sea Oats	Uniola paniculata
Seaside Panicum	Panicum amarum		



Sweet Pepperbush



Green Bristlegrass

